



CITY OF FORT SASKATCHEWAN

BYLAW C14-08

A BYLAW OF THE CITY OF FORT SASKATCHEWAN IN
THE PROVINCE OF ALBERTA TO ADOPT A
DOWNTOWN AREA REDEVELOPMENT PLAN

WHEREAS the *Municipal Government Act* provides that a municipality may adopt an Area Redevelopment Plan;

NOW THEREFORE, the Council of the City of Fort Saskatchewan, in the Province of Alberta, duly assembled, enacts as follows:

1. This Bylaw may be cited as the City of Fort Saskatchewan Downtown Area Redevelopment Plan.
2. The attached Schedule "A" forms part of this Bylaw.
3. If any portion of this Bylaw is declared invalid by a court of competent jurisdiction, the invalid portion must be severed and the remainder of the Bylaw is deemed valid.
4. That Bylaw C14-08 becomes effective upon third and final reading.

READ a first time in Council this 13th day of November A.D., 2008 .

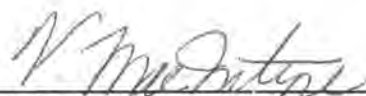
READ a second time in Council this 9th day of December A.D., 2008 .

READ a third time in Council this 27th day of January A.D., 2009 .

SIGNED this 29th day of January A.D., 2009 .

CITY OF FORT SASKATCHEWAN		DATE	INITIALS
City Manager	Principal	Nov 13/08	AS
City Manager	Principal	Nov 12/08	AS
City Manager	Principal	Nov 12/08	AS


MAYOR


DIRECTOR OF LEGISLATIVE SERVICES



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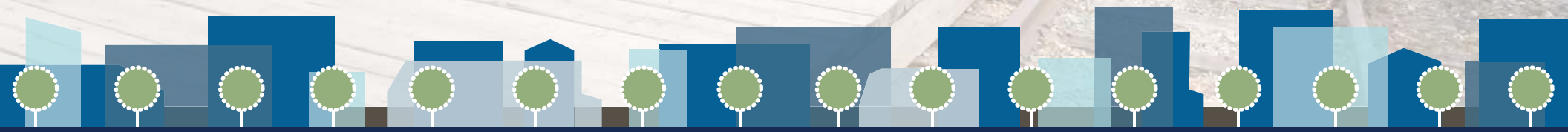


City of Fort Saskatchewan

Downtown Area Redevelopment Plan & Design Guidelines

Bylaw C14-08 "Schedule A"

Adopted January 27, 2009





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CITY OF FORT SASKATCHEWAN			
APPROVED		DATE	INIT.
Dr. Leg.	Legal & Corp.	Nov 12/08	SD
City Mgr.	Content	Nov 12/08	SD
City Mgr.	Principle	Nov 12/08	SD

MAYOR

DIRECTOR OF LEGISLATIVE SERVICES



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ABBREVIATIONS

ARP	Area Redevelopment Plan
BRZ	Business Revitalization Zone
CPTED	Crime Prevention Through Environmental Design
LEED®	Leadership in Energy and Environmental Design
MGA	Municipal Government Act
RCMP	Royal Canadian Mounted Police
VOC	Volatile Organic Compounds





1.0 INTRODUCTION

1.1 CONTEXT

The City of Fort Saskatchewan is at a key moment in its development history. Significant investment and development of the oil and gas industry in northern Alberta has resulted in a pressing demand for downstream processing and distribution facilities. Some of these facilities have located in the industrial area in northeast Fort Saskatchewan, while others are planned for the Industrial Heartland area just outside the northeast boundary of the City.

These developments bring with them both development opportunities and pressures that will have an impact on the community as a whole, including the Downtown area. Fort Saskatchewan's location in the Edmonton Capital Region is shown on Figure 1 - Regional Context.

Fort Saskatchewan's overall population was approximately 14,957 in 2006 (Statistics Canada, 2006 Federal Census), but has risen to an estimated population of 16,793 for 2008 (www.altapop.ca). In addition to its proximity to Edmonton, much of the recent population increases in the City can be attributed to the development of several oil and gas processing facilities and upgraders in the Industrial Heartland area to the northeast of the City. While two facilities have been substantially completed, several others are in the proposal or design stage. When construction on these additional facilities begins, and they

ultimately begin production, there will be additional population increases and resultant development pressures on the City.

Over the past several years, new commercial development on the south side of Highway 15/21 has occurred, resulting in some stagnation of new commercial development in the Downtown core. While some of this commercial development is in the form of large-format retailers, there have been many smaller businesses that have chosen to locate near these large retailers versus in the Downtown area. The location of the Downtown relative to the rest of the City is shown on Figure 2 - Local Context. As a result, many residents in Fort Saskatchewan view the commercial area on the south side of Highway 15/21 as the main retail and service area of the community, rather than the Downtown. While this trend is certainly not unique to Fort Saskatchewan, it has nonetheless resulted in limited new investment by commercial developers and business owners in the Downtown area.





The City has undertaken a number of visioning and consultation exercises in the past few years relating to improvements and attempts to revive investment in the Downtown area, including:

- a Downtown Revitalization Stakeholder Workshop in 2002, conducted by Kaleidoscope Consulting;
- a general public questionnaire relating to the Downtown sent out in 2006, conducted by the Downtown Redevelopment Advisory Committee (DRAC); and
- a vision for Downtown Fort Saskatchewan workshop and development proposal in 2007, prepared by Dr. Avi Friedman and students from McGill University.

However, none of these exercises established a policy direction for future development in the Downtown. To address the opportunities and challenges posed by Fort Saskatchewan's future growth, and to provide policy direction for the area, City Council determined it was appropriate to develop an Area Redevelopment Plan (ARP) for the Downtown area. In addition, City Council also determined that a set of design guidelines for buildings and the public streetscape should be developed for the Downtown. This ARP provides both policy direction to guide future development in the area, and detailed design guidelines to ensure that both the private and public realms are developed in a cohesive, sustainable, and aesthetically-pleasing manner.

The Fort Saskatchewan Downtown ARP area applies to the central area of Fort Saskatchewan (see Figure 3 - Plan Area, and Figure 4 - Aerial Photo). The Downtown area is the oldest and most diverse neighbourhood in the City. Located in the northeast corner of the community, the Downtown is in close proximity to established residential areas, park areas, and the North Saskatchewan River valley system. It also has easy access to Highway 15 to the southwest, and Highway 15/21 to the south.





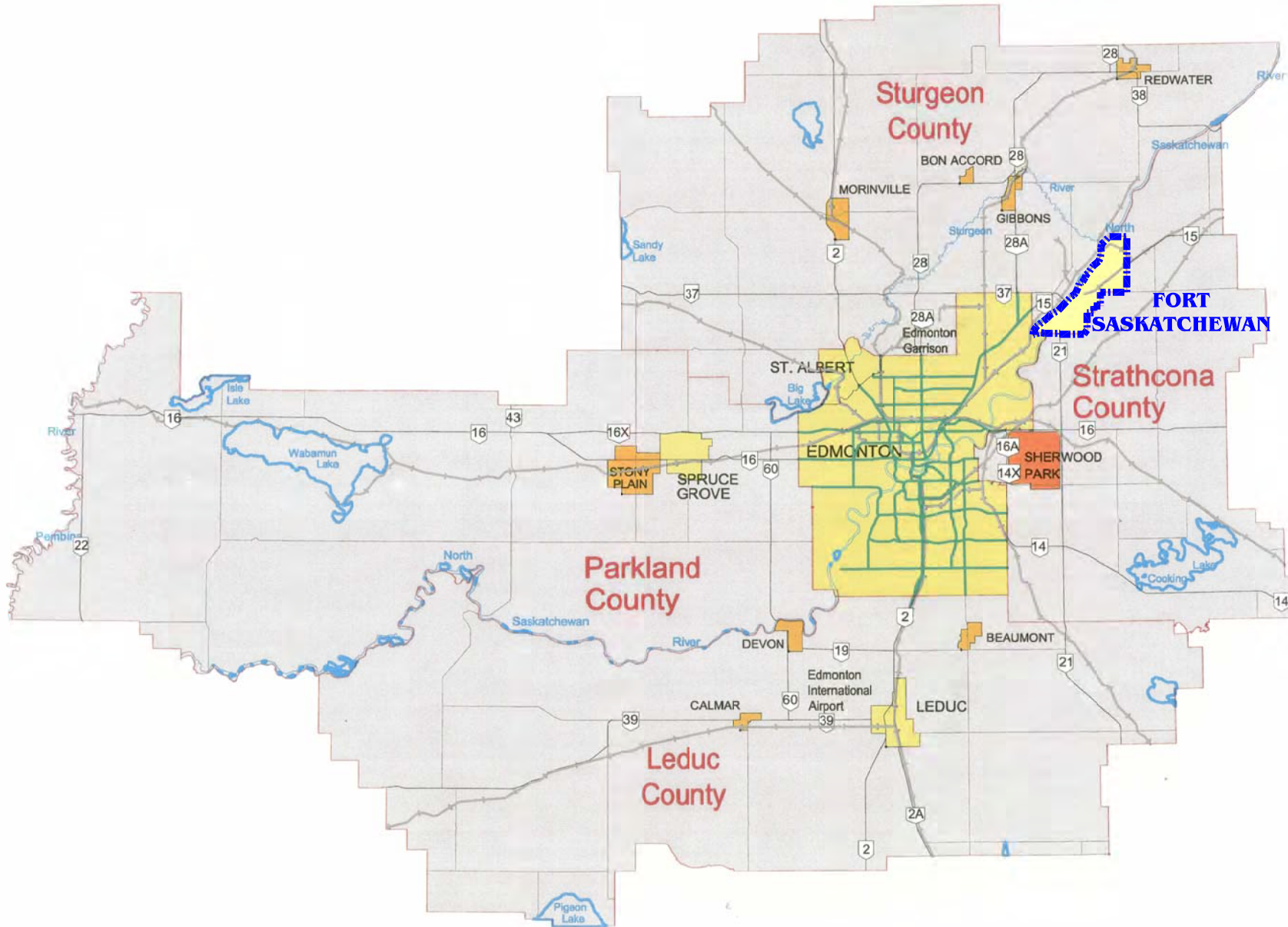
Downtown Area Redevelopment Plan



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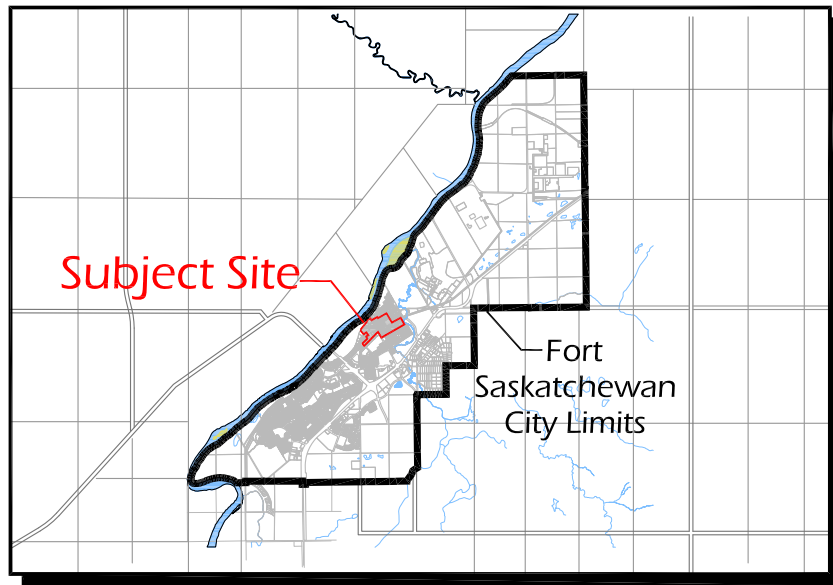


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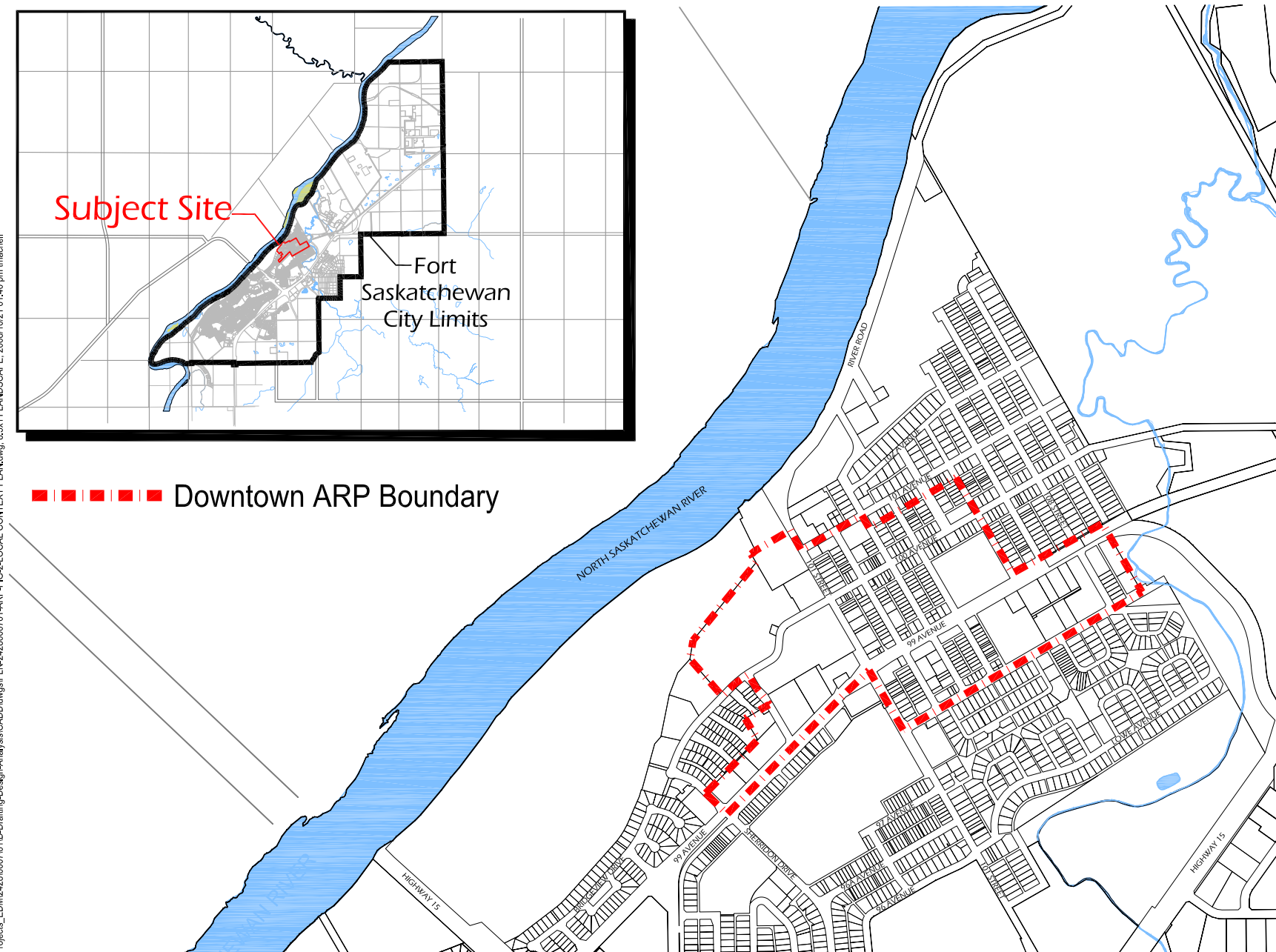


Regional Context

FIGURE
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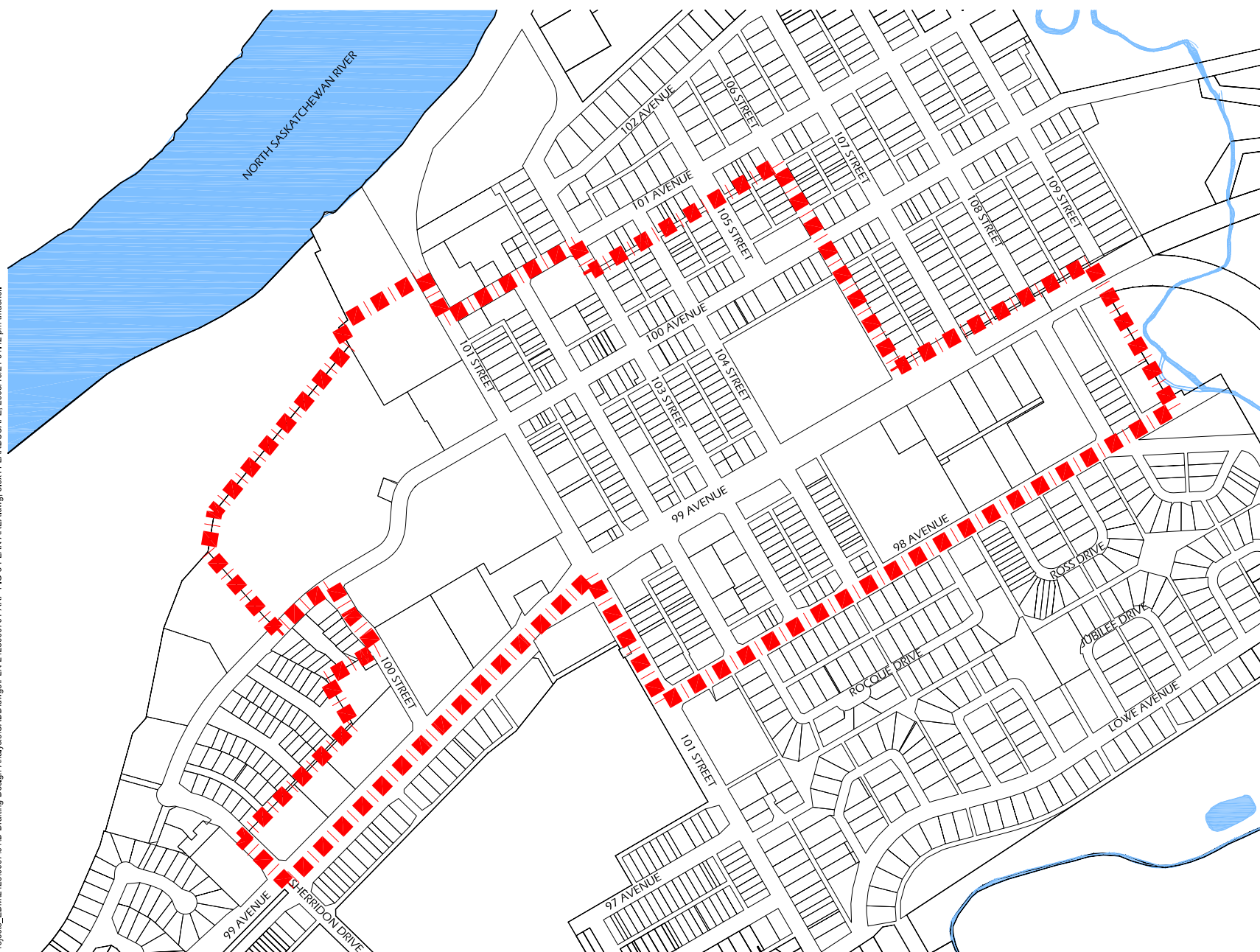
■ ■ ■ ■ ■ Downtown ARP Boundary



Local Context

FIGURE

2



Plan Area

FIGURE

Downtown Area Redevelopment Plan



Aerial Photo

FIGURE

4



1.2 HISTORY OF DOWNTOWN

As the oldest area of Fort Saskatchewan, the Downtown has played a significant role in the formulation of the community over time. In 1875, the Northwest Mounted Police (pre-cursor to the Royal Canadian Mounted Police) established a fort on the banks of the North Saskatchewan River, just to the west of the Downtown, near the present-day Jarvis, Legacy, and Peter Ream Parks. The fort was originally called Sturgeon Creek Post, but was soon after renamed to Fort Saskatchewan. A community began to develop around the fort, and the Village of Fort Saskatchewan was incorporated in 1899.

Fort Saskatchewan was incorporated as a Town in 1904. In 1905, the Canadian Northern Railway reached the community, which resulted in a surge in population and development. In 1915, a new provincial gaol (jail) was built in the town, just west of the existing Downtown area. It acted as a minimum-security prison for northern Alberta, and held prisoners sentenced to less than two years and a day and remands awaiting trial. Twenty-nine prisoners receiving capital sentences were executed at the gaol between 1916 and 1960. Replaced in 1988 by a new facility south of Highway 15, most of the old gaol's buildings were demolished in 1994.

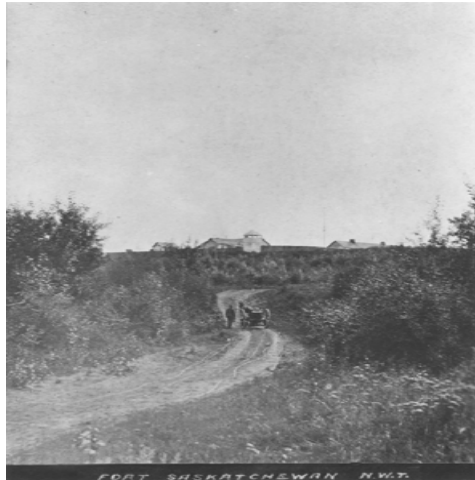
The periods between the First and Second World Wars were relatively quiet for the community. In 1954, Sherritt Gordon Mines Limited established a multi-million dollar nickel refinery in the community, and over the succeeding years Fort Saskatchewan experienced steady growth. Petrochemical industries started to

move into the area, attracted by the availability of land, easy transportation access, salt deposits, abundance of water, a skilled labour force, and an appealing community for their employees. Both light and heavy industries located in the area. The original Canadian Northern Railway station (built in 1905) and a 1957-era caboose are still located in the Downtown, just west of 101 Street, and north of 99 Avenue. The railway tracks along 99 Avenue were removed in 1987. The Fort Saskatchewan Museum and Historic Site, located near Jarvis Park, west of 101 Street, contains the 1937-built Warden's House, as well as the Old Courthouse, built in 1909 (now a Provincial Historic Resource).

In 1985, Fort Saskatchewan became a City. Today, continued investment in the oil and gas industries, and the proximity of the community to the Industrial Heartland area, make Fort Saskatchewan a prime location for growth and development in the foreseeable future. As a result, the Downtown has a tremendous opportunity to regain its role as the centre of a vibrant and strong community.

Existing uses in the Downtown area are shown on Figure 5 - Existing Uses, and the existing zoning in the area, as established by Land Use Bylaw C6-08, is shown on Figure 6 - Existing Zoning.





Trail to Fort Saskatchewan, ca. 1884.



Main Street, ca. 1905.

Fort Hotel in
Downtown, ca.
1955.



Aerial view of Fort
Saskatchewan, 1970.



Entrance sign, ca. 1961.

Public Library,
ca. 1982.



Canadian National Rail
station, ca. 1906.



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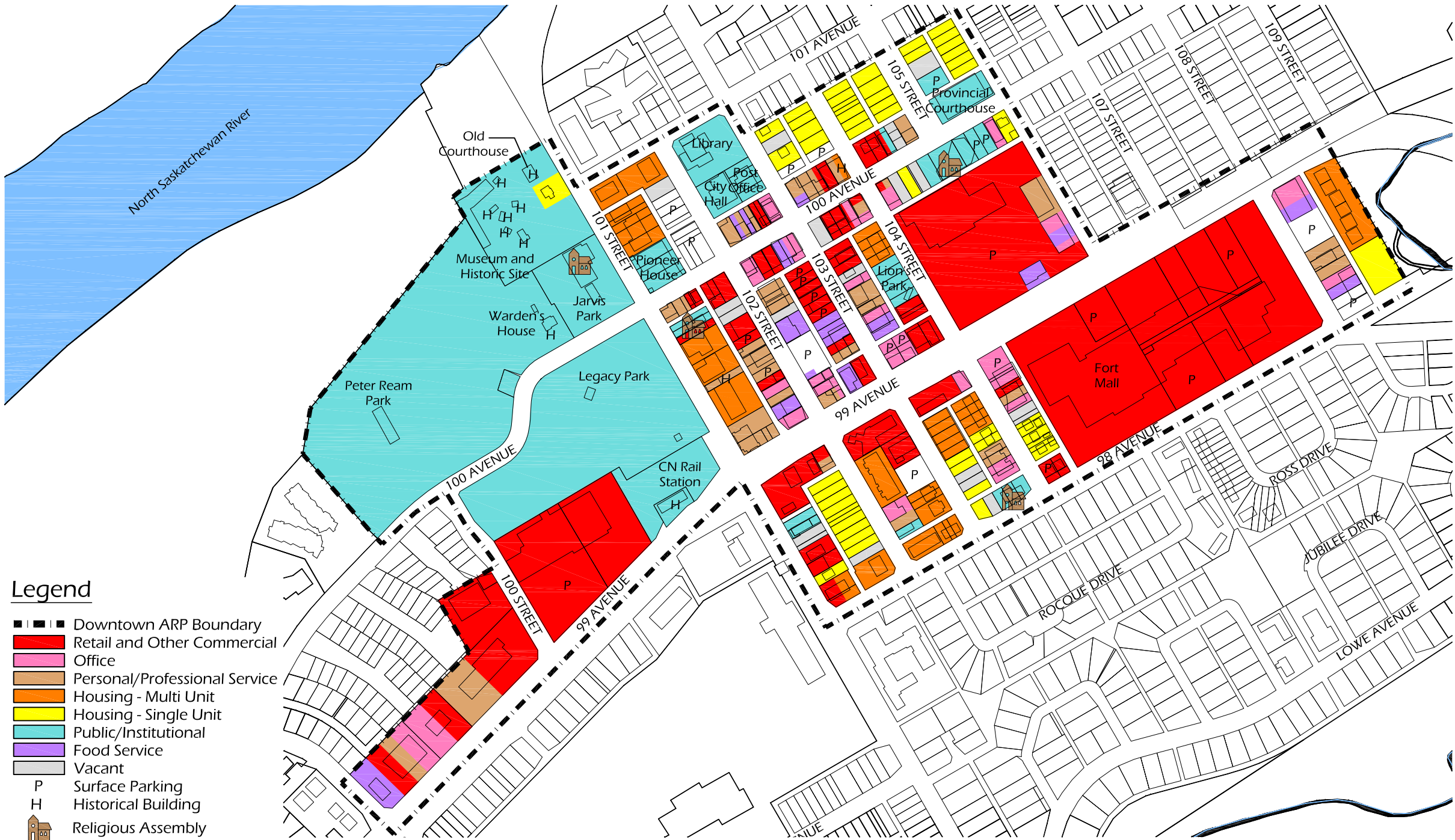


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Legend

- ■ ■ Downtown ARP Boundary
- Retail and Other Commercial
- Office
- Personal/Professional Service
- Housing - Multi Unit
- Housing - Single Unit
- Public/Institutional
- Food Service
- Vacant
- P Surface Parking
- H Historical Building
- Religious Assembly

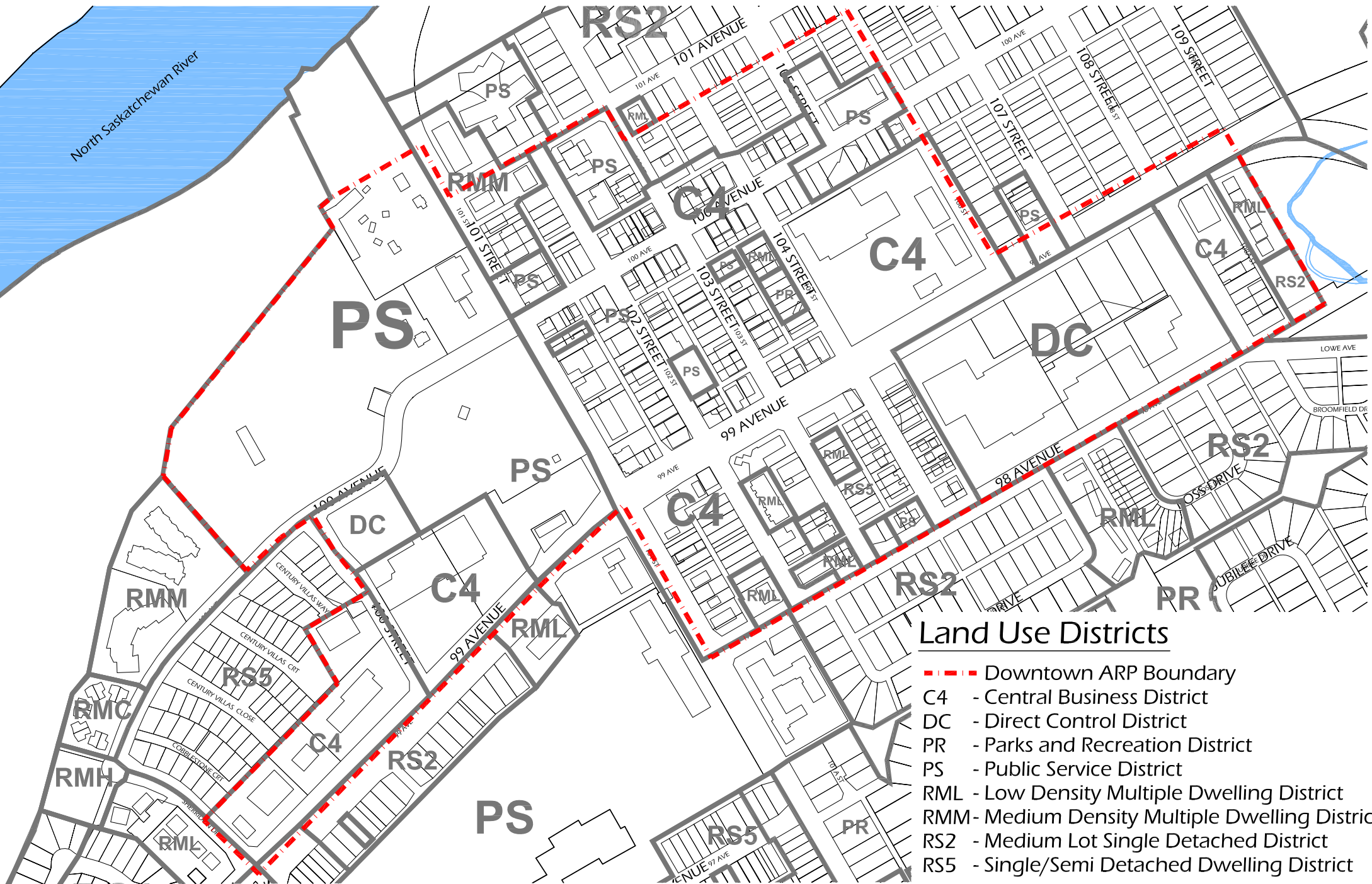


Existing Uses

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- Land Use Districts**
- Downtown ARP Boundary
 - C4 - Central Business District
 - DC - Direct Control District
 - PR - Parks and Recreation District
 - PS - Public Service District
 - RML - Low Density Multiple Dwelling District
 - RMM - Medium Density Multiple Dwelling District
 - RS2 - Medium Lot Single Detached District
 - RS5 - Single/Semi Detached Dwelling District

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1.3 PURPOSE OF PLAN

In 2008, City Council determined that a policy document that applied specifically to the Downtown was needed to guide future growth in the area. The *Municipal Government Act* (MGA) provides a municipality the opportunity to adopt an Area Redevelopment Plan (ARP) to guide future development in a specific area.

In addition to the land use policy direction to be provided in the ARP, City Council also felt it important to establish guidelines regarding building appearance and streetscaping in the Downtown. To address this, City Council determined that specific design guidelines for the Downtown area should also be prepared. The ARP incorporates both the land use policy and implementation information and the design guidelines for each of the sub-areas of the Downtown.



The key objectives of the ARP are to:

- establish a future land use concept for the Downtown area;
- provide policy direction regarding land use, circulation (vehicular and pedestrian), parking, open space, social and cultural issues, sustainability, financing, and economic development, both for the overall Downtown as well as specific sub-areas within;
- provide detailed design guidelines for the various sub-areas of the Downtown;
- identify an implementation program to assist the City in ensuring that the policies and objectives of the ARP are met; and
- address the legislative requirements of the MGA.



1.4 SCOPE

The ARP reflects City Council's approach to new development and redevelopment in the Downtown area through policy and design guidelines that shall guide decision-making, Bylaw development, and future investment in the area.

The ARP has been prepared in accordance with Section 634 of the MGA, which indicates that a Council may designate an area of the municipality as a redevelopment area for the purpose of any or all of the following:

- preserving or improving land and buildings in the area;
- rehabilitating buildings in the area;
- removing buildings from the area;
- constructing or replacing buildings in the area;
- establishing, improving or relocating roads, public utilities or other services in the area; and/or
- facilitating any other development in the area.

The MGA further specifies that an ARP must describe:

- the objectives of the plan and how they are proposed to be achieved;
- the proposed land uses for the redevelopment area;

- if a redevelopment levy is to be imposed, the reasons for imposing it; and
- any proposals for the acquisition of land for any municipal use, school facilities, parks and recreation facilities, or any other purposes the Council considers necessary.

The ARP is intended to guide development activity and reinvestment in the Downtown area, and to provide both short and longer term policy direction. This Plan should be re-evaluated every five (5) years to ensure that it continues to reflect the City's objectives and vision for future development in the Downtown area.

In addition to the five year re-evaluation cycle, the City may find it necessary to amend policies contained within the Plan to address specific matters that may emerge over time.





1.5 PLAN BOUNDARY

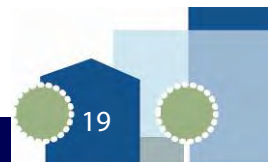
The specific Plan boundaries are shown on Figure 3 - Plan Area. The Plan area has been separated into seven smaller sub-areas, referred to as Precincts, for purposes of providing more specific policy and design guideline direction for these areas. The Precincts are shown on Figure 7 - Precincts, and include the:

- Core Commercial Precinct;
- Live/Work Precinct;
- Mall Redevelopment Precinct;
- Northeast Transition Precinct;
- Civic Precinct;
- Old Fort/Open Space Precinct; and
- 99th Commercial Precinct.

1.6 PLAN PREPARATION PROCESS

The Plan was prepared in consultation with City Council and the City administration. The Downtown Redevelopment Advisory Committee also provided on-going guidance during the preparation of the Plan. The Committee met monthly with the project team, and consisted of the following members:

- Chair, Cindie Hughes, Economic Development Board
- Ed Sperling, Chamber of Commerce
- Harvey Jaehn, Downtown Business Association
- Bill St. Thomas, Fort Saskatchewan Historical Society
- Jim Cockburn, Economic Development Board
- Sandy Harrison, Fort Saskatchewan Historical Society
- Michelle Gamache, Downtown Business Association
- Don Morgan, Economic Development Board
- Carrie Green, Chamber of Commerce
- Mayor Jim Sheasgreen
- Councillor Howard Johnson





Other consultation activities undertaken during the Plan preparation process:

- a public input session on February 21, 2008, to introduce the ARP and design guidelines projects to the community;
- a public Open House on June 18, 2008, to discuss the intent of the ARP, the proposed ARP boundary, the proposed Precincts, the proposed design guidelines, and the initial land use intent for the Plan area with residents of the community;
- a joint session with City Council and the Downtown Redevelopment Advisory Committee on September 9, 2008, to discuss the draft ARP and design guidelines;
- a public Open House on September 15, 2008, to discuss the draft ARP, land use concept, and design guidelines; and
- a Public Hearing, pursuant to the requirements of the *Municipal Government Act*, was held on December 9, 2008, to consider adoption of the ARP bylaw.



Downtown Area Redevelopment Plan



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Precincts

FIGURE

7



1.7 PLAN ORGANIZATION

This Plan is organized into the following Sections:

Section 1 Introduction discusses the purpose and scope of the Plan, provides some background on the Fort Saskatchewan community, and presents a description of the Plan preparation process.

Section 2 Plan Vision provides a vision statement for Downtown Fort Saskatchewan.

Section 3 Plan Objectives and Goals identifies the overall objectives and goals of the Downtown ARP.

Section 4 Overall Policies identifies policy direction that applies to the overall Downtown area.

Section 5 Overall Design Guidelines provides background information on the intent of the guidelines and overall guidelines that apply to the Downtown area.

Section 6 Development Concept introduces the overall land use development concept for the Downtown area.

Section 7 Core Commercial Precinct identifies policy direction and design guidelines for this sub-area of the Plan.

Section 8 Live/Work Precinct identifies policy direction and design guidelines for this sub-area of the Plan.

Section 9 Mall Redevelopment Precinct identifies policy direction and design guidelines for this sub-area of the Plan.

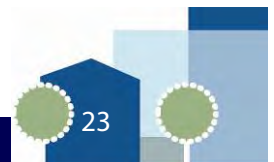
Section 10 Northeast Transition Precinct identifies policy direction and design guidelines for this sub-area of the Plan.

Section 11 Civic Precinct identifies policy direction and design guidelines for this sub-area of the Plan.

Section 12 Old Fort/Open Space Precinct identifies policy direction and design guidelines for this sub-area of the Plan.

Section 13 99th Commercial Precinct identifies policy direction and design guidelines for this sub-area of the Plan.

Section 14 Implementation is the Plan's final section, and provides the necessary tools to ensure that the Plan is effective and relevant in the short, medium, and long terms. It identifies next steps and additional policy development work that will be needed to continue the momentum of the Plan, and to capitalize on the development opportunities that Fort Saskatchewan is being presented.





2.0 PLAN VISION

Downtown Fort Saskatchewan has played an important role as the commercial, social, and administrative centre of the City over the past century. However, over the past several years, the area has lost much of its relevance to the wider community, due to a number of factors. Despite this, the Downtown still retains many elements that can contribute towards its re-emergence as the true heart of Fort Saskatchewan.

Therefore, City Council identifies that the intent of the Downtown ARP is as follows:

To provide direction to ensure that Downtown Fort Saskatchewan is the vibrant heart of the community - the location of choice for business and people, that provides a unique range of residential, commercial, entertainment, cultural, and recreational opportunities, a place where people choose to live, work, and play.

3.0 PLAN OBJECTIVES AND GOALS

- 1) To identify policy direction to assist Council and the City administration in decision-making on proposed developments to achieve the Plan Vision, while remaining flexible to adapt to changing market conditions.
- 2) To re-establish the Core Commercial Precinct as the main commercial/office/retail centre of the Downtown, and the City as a whole.
- 3) To provide opportunities for increased residential development in the Downtown area, specifically in the Mall Redevelopment, Live/Work, and Core Commercial Precincts.
- 4) To accommodate sustainable development initiatives in all aspects of construction and activity in the Downtown to help lessen the ecological footprint of the area.
- 5) To affirm the residential character of the Northeast Transition Precinct, while providing opportunity for limited increases in density and supportive uses in this area.
- 6) To identify significant redevelopment opportunities for the Fort Mall and former Safeway sites in the Mall Redevelopment Precinct, to accommodate high density residential/commercial mixed use development that is sensitive to the scale of surrounding properties.



- 7) To establish design parameters for public and private lands in the Downtown area, to ensure a higher quality and appearance of buildings and streetscapes in each of the Precincts.
- 8) To affirm the role of the 99th Commercial Precinct as the primary vehicle-oriented commercial area serving the Downtown.
- 9) To build upon the tremendous natural and cultural assets of the Old Fort/Open Space Precinct as a major draw to the Downtown area for residents and visitors alike, and to pursue opportunities for additional open space in other areas of the Downtown.
- 10) To improve walkability, the pedestrian-oriented nature of the Downtown, and bicycle connections by promoting linkages between the Precincts and the major features of the area, such as the Fort Mall site and the open space/cultural area west of 101 Street.
- 11) To provide a realistic and appropriate implementation program to assist Council, City administration, business owners, residents, and landowners in achieving the overall intent of the Plan.

4.0 OVERALL POLICIES

4.1 TRANSPORTATION POLICIES

- 1) Designated arterial roadways in the Plan area shall be 99 Avenue, 100 Avenue (east of 101 Street), and 101 Street, as shown on Figure 8 - Transportation and Circulation. Through-traffic should be directed to these roadways.
- 2) Designated collector roadways in the Plan area shall be 98 Avenue and 108 Street. These roadways will handle lower volumes of traffic than the arterial roadways, but will direct traffic from local roads to the arterials.
- 3) The remaining roadways in the Plan area shall be retained at a local road level.
- 4) Continue to accommodate the joint transit service in the Downtown area with Edmonton Transit, and consider the provision of a formal transit hub in the area to help make the system more visible and useable.
- 5) Consider the extension of the Edmonton Transit joint transit service route onto 99 Avenue from 108 Street to 101 Street to serve the future population in the area.





- 6) Encourage an overall community transit system for Fort Saskatchewan to promote transit-oriented development and support intensification of development in the Downtown.
- 7) Periodically monitor traffic flows and movements in the Downtown area to ensure that heavier traffic flows are being directed onto the arterial and collector roadways, and make necessary modifications where warranted.

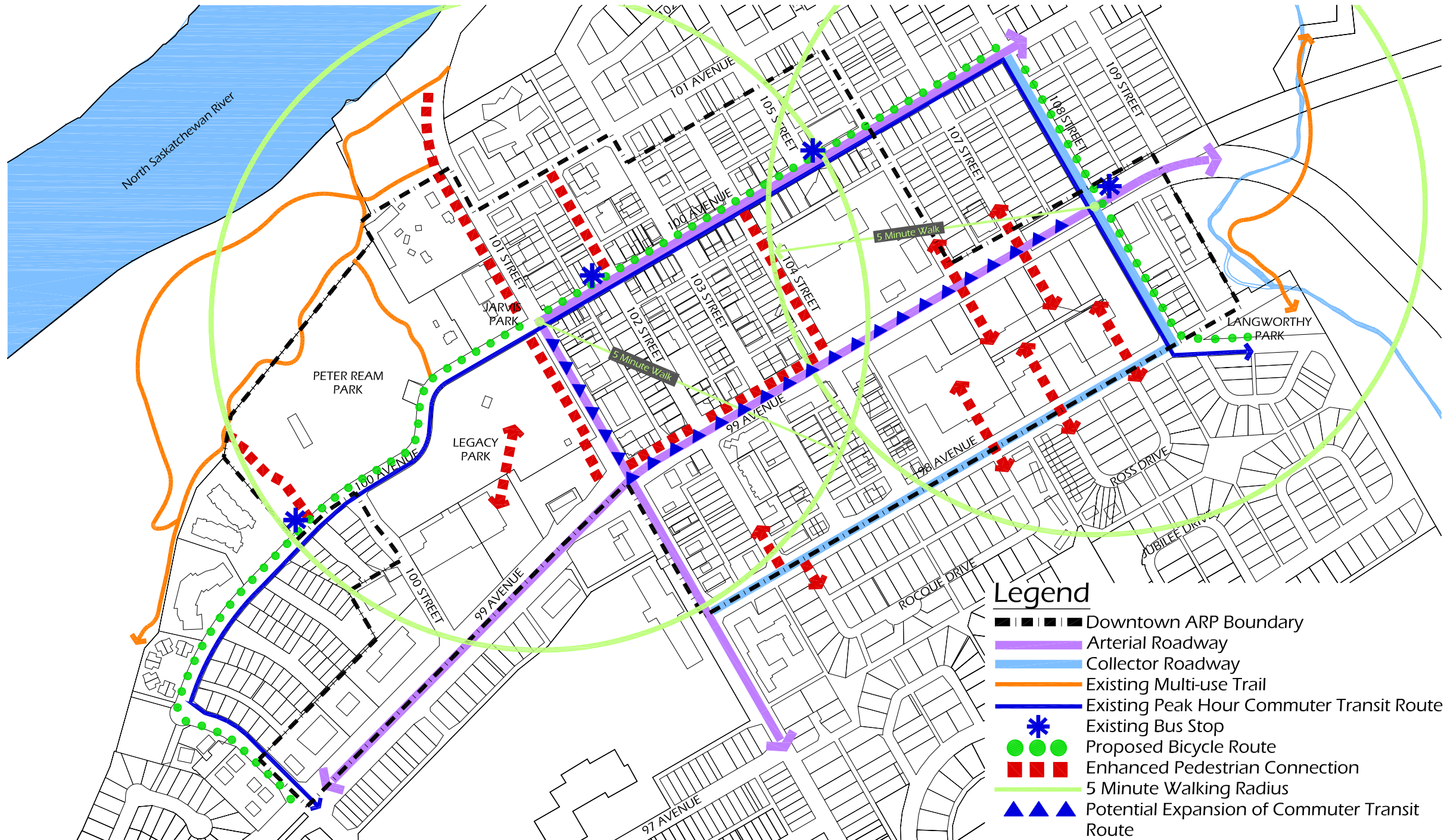
4.2 PEDESTRIAN/BICYCLE CONNECTIONS POLICIES

- 1) Provide improved pedestrian/bicycle linkages to and from the North Saskatchewan River Valley from the Downtown area to encourage users of the trail network to come into the area, as shown on Figure 8 - Transportation and Circulation.



- 2) Ensure new development and major redevelopment projects provide adequate and secure bicycle parking facilities on-site to serve employees, residents, and visitors.

- 3) Adequate and secure bicycle parking facilities shall also be provided by the City at designated locations on public sidewalks.
- 4) Provide a direct, enhanced pedestrian/bicycle linkage through the Downtown area linking the major east/west open space anchors of Langworthy Park to the east and the Jarvis, Legacy, and Peter Ream Parks areas to the west.
- 5) Install wayfinding signage at key intersections in the Downtown which indicate direction and distance to major sites in the area, such as: the Jarvis, Legacy, and Peter Ream Parks; City Hall; the Fort Saskatchewan Public Library; the core commercial area; the 99 Avenue Promenade; the Canadian National Railway Station; the Fort Saskatchewan Museum and Historic Site; and the North Saskatchewan River Valley, among others.
- 6) Encourage new street-level commercial developments, particularly in the Core Commercial and Mall Redevelopment Precincts, to provide outdoor amenity areas for patrons and visitors, such as seating areas and plazas to create better interaction with the abutting sidewalk and street.
- 7) Ensure that the location of street furniture installed along public sidewalks does not restrict universal accessibility for all users.





4.3 VEHICULAR PARKING POLICIES

- 1) Encourage new developments to provide the required on-site parking identified in the Land Use Bylaw underground. If this can be demonstrated as not being feasible, surface parking areas on site shall only be permitted at the rear of buildings, and shall not be developed adjacent to any public roadway, other than a lane, unless a suitable interface with the abutting street, including substantial screening of vehicles, is possible.
- 2) Consider the development of an additional municipally-owned and operated parking facility in the Downtown area.
- 3) Consider the establishment of a program where money may be provided to the City in lieu of required on-site parking. Funds collected through this program would be assembled for use towards the development of a municipally-owned and operated parking facility in the Downtown area.
- 4) Require owners of existing surface parking lots in the Downtown area to bring these facilities into compliance with the standards of the Land Use Bylaw, particularly regarding hardsurfacing and landscaping.
- 5) Retain the on-street, curb-side vehicular parking spaces in the Downtown to ensure an adequate supply of parking for those shopping in and visiting the area.
- 6) Consider proposals for the reduction of required on-site vehicular parking for new developments, redevelopments, or additions or enlargements to existing buildings, in accordance with the parameters outlined in Section 2.33 of the Land Use Bylaw.
- 7) Surface parking areas should provide on-site stormwater management storage and treatment facilities to reduce the amount of and improve the quality of stormwater runoff from hard-surfaced areas.





4.4 STREETScape IMPROVEMENTS POLICIES

- 1) Develop a consistent streetscaping plan that identifies new streetlights, sidewalk treatments, landscaping, and street furniture that is unique to the Downtown, as shown on Figure 9 - Open Space and Streetscaping.
- 2) Streetscape improvements, and in particular new street furniture, should not impede the movement of pedestrians, and should adhere to Crime Prevention Through Environmental Design (CPTED) principles and techniques to create a safe environment.
- 3) In conjunction with a monitoring program of traffic volumes on Downtown streets, identify and implement additional pedestrian crossing measures to ensure the pedestrian-oriented environment in the area is maintained.

- 4) Through streetscaping improvement projects, identify locations for new boulevard trees to improve the appearance of



Downtown streets, while taking into consideration the location of adjacent businesses' signage, as shown on

Figure 9.

- 5) Consider locations in the Downtown to widen sidewalks as part of any new streetscaping initiative to allow better pedestrian movement and opportunities for active uses such as patios, plazas, and outdoor seating areas.
- 6) Consider the installation of decorative banners on streetlights in the Downtown that highlight the unique features and history of the area.
- 7) Consider the installation of an entrance feature at a key entry point(s) into the Downtown to identify the area as a unique place to visitors.
- 8) Ensure that the existing sidewalks, streets, and other public spaces in the Downtown get higher priority for maintenance to maintain a high level of appearance for the area.
- 9) New projects and significant redevelopment projects may be responsible for the costs of identified streetscape improvements to the portion(s) of road right(s)-of-way abutting a site.
- 10) Existing mature boulevard trees along streets and avenues in the Downtown area should be preserved.

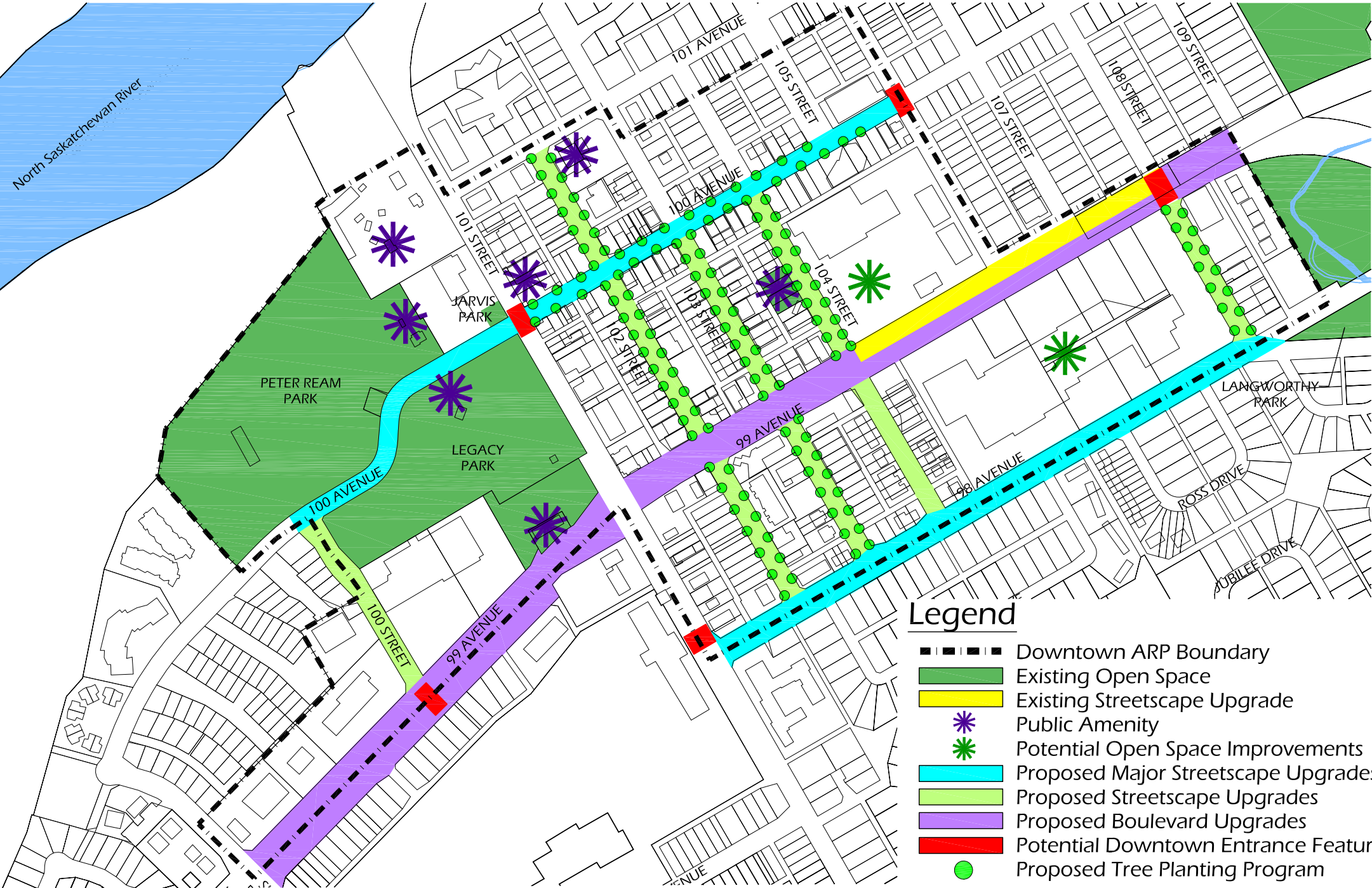




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Open Space and Streetscaping





4.5 OPEN SPACE POLICIES

- 1) Pursue opportunities for the City to acquire individual lots in the Downtown, specifically in the Core Commercial Precinct, with the intent to provide small, landscaped pocket parks that provide opportunities for passive recreation.
- 2) For sites over 2.0 ha in size, encourage the provision of a minimum of 15% of the overall site area to be set aside as publically-accessible open space that is connected to the wider pedestrian network, as part of any redevelopment. Open space should be developed to City standards, and provide one (1) tree per 25 m² of site area.
- 3) Maintain and enhance the open space experience along the bank of the North Saskatchewan River valley and the Jarvis, Legacy, and Peter Ream Parks for use year-round, as shown on Figure 9 - Open Space and Streetscaping.
- 4) Provide additional linkages from the Downtown to the North Saskatchewan River valley to promote additional usage for walking, running, in-line skating, bicycling, cross-country skiing, or other modes of pedestrian-oriented transportation, as shown on Figure 8.



- 5) Identify opportunities to provide additional outdoor seating areas for the public along streets in the Downtown, particularly in the Core Commercial, Civic, and Mall Redevelopment Precincts.
- 6) Encourage the continuation of the Legacy Park Snowbank Rink and support additional outdoor use of the open spaces in the Downtown in the winter months.
- 7) Maintain and enhance the Lion's Park playground and outdoor wading pool for use in the spring, summer, and fall.
- 8) Encourage the retention, to the extent possible, of existing trees in the Downtown area as part of any new development or redevelopment project or improvement project to public street.
- 9) Consider the expansion of the Turner Park Campground in the North Saskatchewan River valley, promoting the proximity of the facility to the Downtown, and enhancing linkages between the two.
- 10) Explore opportunities for youth-oriented recreation options in the Downtown area.





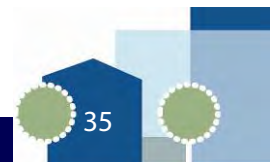
- 11) Consider the adoption of a redevelopment levy in the Downtown area to raise the necessary funds for the acquisition of additional public open space, in accordance with Section 647 of the MGA.
 - 12) Benches, pedestrian lighting, street furniture, and paving treatment shall be incorporated in clusters that are integrated with the overall site planning and building architecture.
 - 13) Universally accessible design should be considered in the design of open spaces where economically and physically feasible. Development should be designed to be accessible to persons in wheelchairs, motorized scooters, strollers, and persons with special needs.
 - 14) Public parks shall be completed to base level development (grade, level, sod, and trees at 50 trees per ha).
- #### 4.6 SUSTAINABILITY POLICIES
- 1) Encourage planting additional trees along public roadways and on other public lands, as well as part of on-site landscaping for new developments or major redevelopments to help improve the ecological health of the Downtown.
 - 2) Ensure that an adequate supply of waste receptacles and recycling collection stations are provided along public sidewalks and near commercial frontages to assist in waste management efforts in the Downtown.
 - 3) Continue and enhance the City's curbside recycling blue-bag program throughout the Downtown area, and ensure that it is extended to all new developments.
 - 4) Promote non-vehicular modes of transportation in the Downtown to improve air quality and limit noise pollution through the provision of additional pedestrian and bicycle trails, linkages, and other facilities.
 - 5) Explore opportunities to reduce energy consumption in the Downtown area through the establishment of a district heating or geothermal heating system and energy-efficient lighting.
 - 6) Provide alternate and innovative approaches to stormwater management treatment for new developments, major redevelopments, and existing development, to improve the quality of runoff entering the North Saskatchewan River.
 - 7) Promote water conservation measures through water usage in buildings and on-site landscaping.



- 8) Consider the development of a “green initiatives” program through the Land Use Bylaw that provides opportunities for intensification of development or other incentives to developments that meet pre-determined criteria.
- 9) Support proposals for live/work mixed-use developments, particularly in the Live/Work Precinct, that allow people to work and live in the same building, thus reducing or eliminating the need to commute.
- 10) Complete energy audits on all City facilities to ensure they are operating efficiently and in an environmentally-friendly manner.
- 11) Encourage the use of diverse native plant species in required landscaping features as part of any development.
- 12) Encourage the use of permeable surface materials for parking or other hardsurfacing areas as part of public improvements or private redevelopment.

4.7 ECONOMIC DEVELOPMENT POLICIES

- 1) Encourage a broad range of business and employment opportunities to locate in the Downtown, and strive to make the area the “location of choice” for new businesses in the City.
- 2) Encourage partnership between the Downtown Business Association, the Chamber of Commerce, and the Economic Development Board to focus efforts on attracting new development to the Downtown.
- 3) Consider the establishment of a Downtown Design Committee, whose role would be to review proposals for new development and significant redevelopment proposals from an urban design perspective, and provide feedback to the City administration.
- 4) Work with business owners to re-establish a Downtown Business Revitalization Zone (BRZ) in the area, in accordance with Section 50 of the *Municipal Government Act* and the *Business Revitalization Zone Regulation* (Alberta Regulation 377/94).
- 5) Develop a municipal grant program that provides financial assistance to property or business owners who wish to undertake improvements to their building’s façade.
- 6) Consider a financial incentive or possible tax relief for projects that provide new residential development, and in particular, affordable housing units, in the Live/Work, Mall Redevelopment, and/or Core Commercial Precincts.
- 7) Consolidate City administrative staff in a central location in the Downtown to create efficiencies and more convenience for the public and those doing business with the City.





- 8) Explore the opportunity for lowering the property tax rate for Downtown businesses.
- 9) Consider waiving business license fees for businesses that are located in the Downtown.
- 10) Consider the development of a Downtown Investment Fund that collects monies from the money-in-lieu of parking program identified in Policy 4.3.3 above, or funds that Council directs for Downtown-specific projects.
- 11) Consider the implementation of a redevelopment levy that would be collected from new developments towards the purchase of additional park space in the Downtown, further to Section 647 of the *Municipal Government Act*.
- 12) Create a special initiative between the City, the Downtown Business Association, and the Chamber of Commerce that focuses on the branding and image of the Downtown as a place to live, work, and do business.
- 13) Undertake a market research study to identify specific opportunities for commercial and residential development in the Downtown.

4.8 ARTS, CULTURE, AND SOCIAL POLICIES

- 1) Pursue opportunities to locate new or existing municipal and private arts and culture facilities in the Downtown.
- 2) Establish a Public Art Program that would require 1% of the construction costs of new public or private developments in the Downtown to be provided towards the purchase and installation of public art on a site.
- 3) Encourage public/private partnerships to increase the amount and profile of advertising of arts and culture, and other community events in the Downtown.
- 4) Continue to support and enhance the use of the Legacy Park Band Shell as a venue for live entertainment and other gatherings.
- 5) Retain and enhance the Pioneer House facility and site as a meeting place for seniors and a community gathering place in the Downtown.
- 6) Consider the establishment of a satellite gallery in the Downtown of the Alberta Lottery Fund Art Gallery that is currently located in the Sherritt Cultural Pavilion at the Dow Centennial Centre.





- 7) Extend cultural activities such as the Canada Day parade into the core area of the Downtown from the Old Fort/Open Space Precinct to promote linkages between the two areas.

- 8) Support the development of a Downtown Mural Program to provide opportunities for local artists to install murals on blank walls of selected buildings.



- 9) Support and look for opportunities to expand the Farmer's Market program at Legacy Park, but also consider the relocation of the Market (or an extension of the Market) to the Downtown core along 102 Street or 103 Street between 99 and 100 Avenues, with associated temporary street closures to accommodate pedestrian traffic.
- 10) Consider developing a municipal heritage inventory or designation program to assist in maintaining and preserving the Downtown's historical assets.
- 11) Support and enhance the Fort Saskatchewan Museum and Historic site facility as an attraction for visitors and residents alike that provides connections to the community's past.

- 12) With the Fort Saskatchewan Historic Resources Advisory Board and the Historical Society, conduct a detailed heritage inventory of the entire area to identify existing and potential historic resources that reflect the evolution of the Downtown over the past century.

- 13) Identified historic resources should be protected and enhanced, through adaptive re-use.

- 14) Hire a full or part-time "Downtown Development Coordinator" position to focus specifically on Downtown issues, working closely with the Downtown Business Association, the Chamber of Commerce, and the Economic Development Board.





5.0 OVERALL DESIGN GUIDELINES

5.1 BACKGROUND

The Downtown of any community plays a unique role in its overall image, and Fort Saskatchewan is no exception. While actual land uses help define people's perception of the Downtown, the physical appearance, both of its buildings and its public infrastructure, also plays a key role in forming how people view the area.

To reflect a compact, sustainable, and pedestrian-friendly Downtown, new developments and redevelopment of existing buildings should adhere to the following guidelines in conjunction with the other policies in this Plan, and in coordination with their individual context and public realm improvements.

The buildings and public space in Downtown Fort Saskatchewan are to create a unique character for the area, while being responsive to the surrounding neighbourhoods.



5.2 BUILDINGS AND SITES

Street Wall

Conventional city streets are characterized and defined by 'street walls', the lower façades of buildings, especially the first three storeys. Typically, street walls are building fronts, parallel to the right-of-way, and are built up to and along the frontage or front property line setback.

There is a greater sense of street wall where building frontage onto the street is continuous, and gaps between the buildings are eliminated. Street walls enclose the street.

Well-proportioned, they engender comfort to the users. Their continuity contributes to the vitality and life of the streets, offering "eyes on the street". Where gaps and open spaces disrupt the defining street walls, the streets lose their sense of enclosure.

5.2.1 Street Wall Height

Street wall heights within the Downtown are to be a minimum of 2 storeys and a maximum of 4 storeys.

Discussion:

- *The street walls are the prominent or principal faces of the buildings lining the sides of a street. Their heights effectively establish the height of the street space.*



5.2.2 Street Wall Definition

Portions of buildings which are higher than 4 storeys are to be stepped back to ensure the visual primacy of the street wall.

Minor step-backs, projecting cornices, changes of material or other architectural elements are to define the street wall of taller buildings.

Streetwalls along streets with a predominance of commercial retail activity are to be highly transparent and articulated at ground level to engage pedestrians and offer a high level of amenity and interest.

Discussion:

- *Commercial and retail shops are to have transparent windows located along the street, conducive to public viewing. Transparent display windows are to account for 60% of the associated wall area adjacent to the street.*
- *Small variances are to be incorporated into building façades to avoid long expanses of uninterrupted glass or other building materials. These variances can be achieved with devices such as bay windows, small setbacks, and a variety of building materials.*

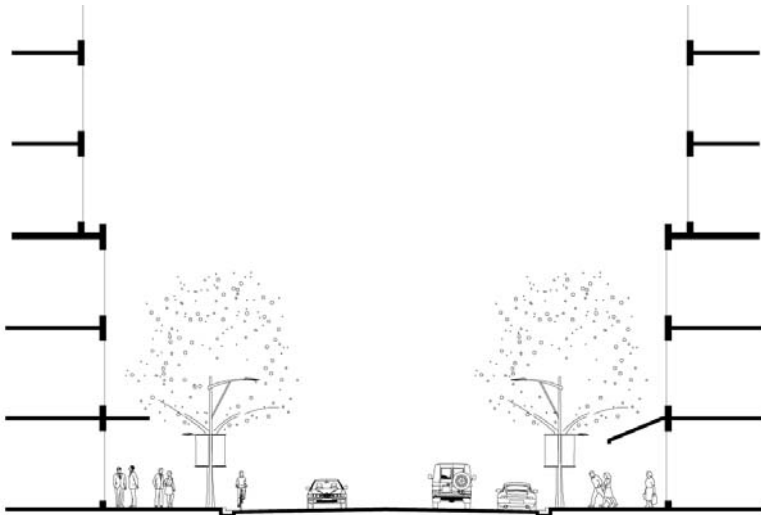


Building frontage onto the street is continuous. Commercial retail stores on ground floor are to be highly transparent to engage a high level of pedestrians and offer amenity and interest.



5.2.3 Building Height and Massing

Base building height is to be a minimum of 2 storeys and a maximum of 4 storeys as measured along the street.



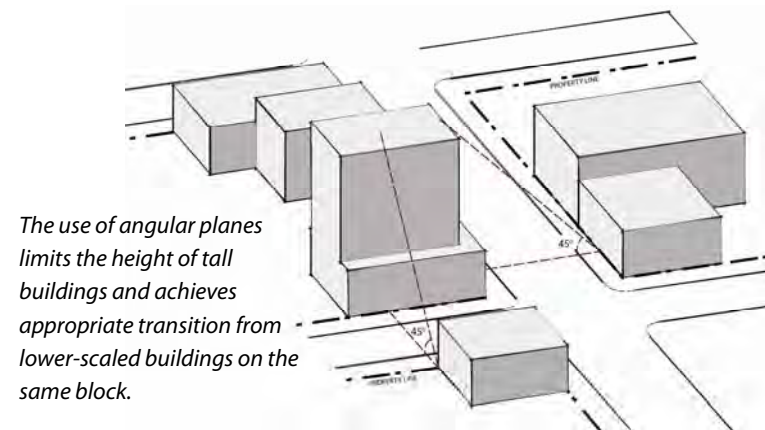
The base building height is to be no more than 4 storeys.

Buildings are to be designed to provide a height transition to surrounding higher or lower scale developments and public spaces to minimize impacts of taller buildings, such as shadowing and wind acceleration on surrounding residential areas.

In the Mall Redevelopment Precinct, buildings are to use a 45° angular plane from street level to determine building height and required setbacks for upper portions of buildings on a specific site.

Discussion:

- Utilizing a site cross-section, a 45° angle measured from the property line located directly across the street for a proposed development and/or from the closest property line of sites with lower-scaled buildings, determines the building height along that frontage. Additional building height can be achieved with set backs of upper portions.
- The 45° angular planes ensure compatibility of buildings, adequate view of the sky, and adequate sunlight reaching the sidewalk.



Buildings of over 15.0 m in frontage length are to be designed to offer interest to pedestrians through the articulation of façade elements.

A minimum setback of 1.0 m, measured from the front face of the lower portion of a building, is required, for portions of any buildings above 4 storeys.



Discussion:

- *Setbacks are to be incorporated for floors above the lower portion of buildings to minimize the perception of upper storeys from the vantage point of a pedestrian at street level.*

5.2.4 Setbacks

Where applicable, buildings are to be built up to the front property line (non-residential space) or to a setback line (residential space) to provide enclosure to the street space.

Discussion:

- *Non-residential developments are to be built up to the front property line or a required setback line, where a wider sidewalk is necessary for the placement of pedestrian amenities.*
- *Selective commercial/retail building setbacks not exceeding 3.0 m from the front property line will be considered, provided that the area within the setback is to accommodate active space such as cafés, arcades, or plazas.*
- *Residential developments are to be set back a maximum of 3.0 m from the front property line and raised a maximum of 1.0 m above street level. The setback is to accommodate appropriate landscaping and amenity space for the abutting dwelling unit such as patios, courtyard, or terraces.*



Setbacks for commercial establishments on the ground floor are used to accommodate active retail activities.

Commercial Streets

The space between the back of walk to a building wall on commercial streets, is to have a walking surface and planting that is coordinated with the City sidewalk (both paving and grading).

A wider sidewalk allows a number of retail activities to occur to enliven the pedestrian environment, and better serve heavier pedestrian volumes.

Retail activities are encouraged to utilize the sidewalk immediately outside their retail frontages, provided it does not interrupt the flow of pedestrians. Desirable activities include outdoor cafés, produce stands, and merchandise displays.

Setbacks for long buildings are to be varied and intermittent to animate the street and related individual store front entrances.

Setbacks for amenities are to be paved as an extension of the sidewalk, with minimal changes in grade. Where space permits, a row of trees should be planted along the setback, flush to the level of the sidewalk.



Residential Streets

- Substantial amounts of planting, in the form of a display garden, are to be introduced to provide a degree of privacy from adjacent sidewalks.



The ground floor of the residential unit is set back 3.0 m from the front property line, and raised 1.0 m above the street level to accommodate landscaping and maintain “eyes on the street”.

5.2.5 Corner Lots

Buildings on corner lots will front both streets, and distinguish the street intersection to give it prominence.

Discussion:

- *Developments on corners should acknowledge the intersection of the adjoining streets through their siting and massing. Buildings at the street intersections should be sited by building parallel to both streets up to the corner, while providing a corner cut to the building to maintain sightlines. This siting strengthens the streetwall at the corner and gives greater visibility and prominence to both the building and the intersection. Since the characteristics along each of the intersecting streets may be different, building design and massing should reflect differing setbacks.*

Buildings on corners are to have special architectural treatment and emphasis.

Discussion:

- *Use articulated, or special wall or roof line features to emphasize their prominent location.*
- *Exploit special landmark opportunities of particularly prominent corners by using a higher corner building element, changes of material, and/or retail emphasis of the corner.*
- *Site public art elements to mark and articulate special corner sites.*



This corner lot fronts both the street and the avenue. The roof line of the building is architecturally articulated and the entrance to the ground floor is highlighted through the use of a canopy, emphasizing a distinguishable retail establishment.

5.2.6 Architectural Components

Entrances

Buildings are to address the adjoining street with entrances that are clearly visible to lend a sense of address for buildings, dwelling units, and storefronts.

Buildings are to provide entrances facing adjacent streets and public pedestrian areas, not onto nearby service areas.

The sense of arrival to a building is expressed through the design and detailing of its entrance, such as the use of canopies to indicate entrances and provide weather protection.

Entrances are to be enhanced through the use of architectural and landscape devices such as benches, low walls, steps, a variety of paving materials, planting features, architecturally-integrated canopies projecting from the building (which may project into the required setback), and architectural lighting.

Street entrances are to be designed for universal accessibility.

Discussion:

- *Consideration should be given to maximize accessibility to buildings for wheelchair users, the elderly, people with disabilities, and people using baby strollers. Level changes from the sidewalk to entrances of public buildings will be minimized. Sidewalk furniture and other elements should be located out of the travel path to ensure they are not obstacles.*



The entrances to this building are facing the adjacent street, universally-accessible, and are clearly indicated through the use of canopies and other signage.



Four types of building entrances are described by the design guidelines:

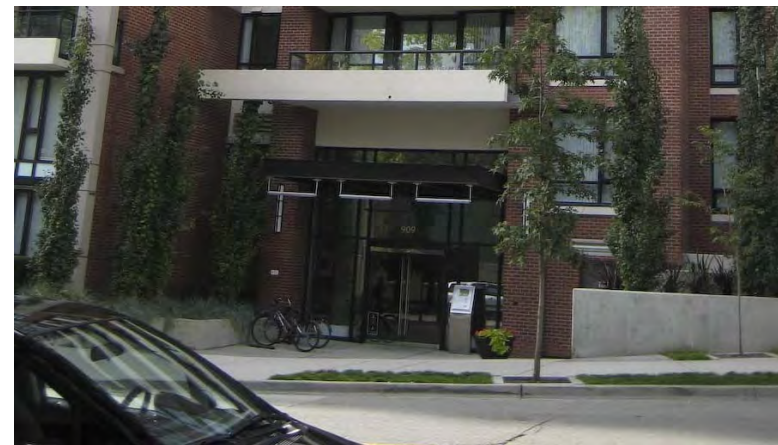
- 1) Individual residential entrances (associated with access to townhouses or other individual residential units, either freestanding or at the base of buildings):
 - a. Ground floor residential units are to have individual entrances to the street to emphasize the residential nature of the area.
 - b. Residential units located on the ground floor are to establish the main floor at a maximum of 1.0 m above the adjacent street level to promote privacy for the units yet still maintain “eyes on the street” from the unit. These entrances are to be spaced frequently and combined with steps, terraces, or stoops.



Individual entrances into residential units on the ground floor are set back 3.0 m from the property line and are raised 1.0 m above the street level to accommodate landscaping and maintain “eyes on the street”.

- 2) Residential lobby entrances (associated with access to multi-unit residential buildings):

- a. Ground floors are to be level with the grade of the adjacent sidewalk, or accessed by a ramp in order to be universally accessible.



This residential lobby entrance is level with the sidewalk grade and is universally accessible. The entrance is highlighted with the use of a canopy and landscaping.



3) Commercial retail, office and live/work entrances:

- a. Building entrances are to be located at intervals of every 6.0 - 10.0 m along building façades facing onto or visible from streets and public spaces.
- b. Ground floors are to be level with the grade of the adjacent sidewalk and universally accessible.
- c. Individualized entries to retail, restaurant, and service establishments are to be provided immediately off the street.
- d. Building entrances are to be visible from adjacent streets, and highlighted through the use of architectural and landscaping devices such as benches, low walls, steps, a variety of paving materials, planting features, architecturally-integrated canopies projecting from the building (which may project into the required setback), and architectural lighting.
- e. Entrances to large establishments are to be located away from corners where pedestrian congestion may already exist. Corners are best reserved for retail uses.

4) Large scale, mixed-use development entrances:

- a. On development sites with long frontages, frequent entrances to stores, offices, and residential uses are encouraged to create a more animated, social, and safe street environment. These entrances are to be located at intervals of every 6.0 - 8.0 m along building façades facing onto or visible from adjacent streets and public spaces.
- b. Residential and commercial entrances are to be separated and architecturally-differentiated from each other.



Building entrances are located at intervals of every 6.0 - 8.0 m along the building façade. The entrances are highlighted through the use of accent materials, planting, canopies, and architectural lighting.



Awnings, Canopies, Recesses, and Arcades

Non-residential space at street level is to provide weather protection for pedestrians along adjacent streets and public access routes.

Discussion:

- *Protection from wind, rain, and hot sun is essential to provide pedestrians with a comfortable environment.*
- *Weather protection over sidewalks and entrances is to be provided as part of commercial retail frontage.*
- *Weather protection is also encouraged in non-landscaped areas where the public might congregate.*
- *Canopies are to:*
 - *be wide enough to shelter people from the weather;*
 - *be mounted within the first floor level of a building;*
 - *be designed to manage snow buildup;*
 - *be designed to reflect the building's architectural style, giving consideration to integrating signage and lighting; and*
 - *have supports that are slender, attractive, and located to minimize impacts on pedestrian traffic.*

Transparent canopies are to be used for the shady side of streets to offer more direct light to the sidewalk.

Continuous weather protection is required for commercial frontages on 99 Avenue and 100 Avenue, in the form of awnings or canopies. These are to be of sufficient depth (minimum 1.5 m) to shelter outdoor display and seating, as well as to protect walking space on the adjacent public sidewalk.

These canopies are an integral part of the building, providing weather protection with integrated signage. The supports are detailed to form an interesting street-level façade.





Exterior Walls and Finishing Materials

Lower Floors of Buildings:

Discussion:

- *The lower floors of buildings form part of the streetscape, and are important in shaping the public realm and creating visually-interesting and active pedestrian environments.*

No building wall shall extend beyond 10.0 m horizontally without appropriate façade variation or articulation.

Discussion:

- *Devices such as changes in materials, cornice lines, and changes in fenestration scale are to be used to achieve a comfortable pedestrian scale at the lower levels of buildings.*
- *Building walls are to be visually interesting and provide variation in both the horizontal and vertical wall plane through:*
 - *the use of multiple at-grade, accessible building entrances and large window openings (views to interior and/or for displays) at a consistent rhythm to create a visually-interesting and active pedestrian environment; and*
 - *the installation of a mixture of functional building elements and architectural details, such as lighting, signage, hanging planters, banner flags, friezes, canopies, colonnades, etc.*

- *Richer materials, more intensive decorative details, and lighting are to be used to enhance the “close up” view of a building for the pedestrian. In the case of retail uses, display windows, lighting, outdoor display, and weather protection are to be incorporated.*

Colour and Materials:

Where an existing building is adjacent or nearby, the new development need not match that building, but should be designed to ensure visual compatibility between them.

Richer materials, such as brick, glass, and steel are to be predominantly used on the base of buildings.

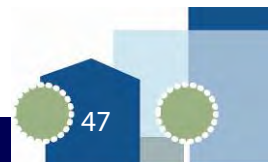
Retail Frontages:

Where retail is permitted, small, individual store frontages not exceeding 8.0 m in width are to be provided.

Where a large tenancy is planned, the majority of its area is to be located behind smaller tenancy frontages, without creating an internalized “mall”.

Display windows and individualized tenancy design are to be used to enhance pedestrian interest.

Façades facing a public street or public area are to incorporate a minimum of 60% glazing at grade and on the first two storeys to encourage pedestrian interaction and safety.





Discussion:

- *Transparent areas allow views into and out of the structure, or into display windows from the outside.*



The lower floor of this building utilizes richer materials and large display windows to create a visually-interesting and active pedestrian environment.



The large tenancy in this building is located above smaller tenancy frontages that are highly transparent. Entrances to smaller tenancies are provided at intervals of 6.0 - 10.0 m.

Balconies

Private balconies, outdoor terraces, and bay windows are to be integral to the overall form and design of buildings, particularly residential buildings.

Wherever possible, balconies are to be provided for all residential units.



The balconies project a maximum of 1.0 m from the building face. An outdoor terrace above the ground floor provides additional amenity space for occupants.



In relation to the street walls, balcony design is to be guided by the following:

- balconies on the street walls are to be partly or fully recessed from the building face with approximately 50% of their perimeter contained by the exterior walls of the building;
- french balconies and bay windows are encouraged on street walls (french balcony guardrails are to be predominantly open rail or glass to maximize natural light penetration);
- balcony projections are not to project beyond the front property line; and
- projections of balconies or terraces on street walls are to be limited to a maximum of 1.0 m from the building face.

Private terraces are encouraged at the set backs of the upper levels of buildings, and in combination with the stoops of townhouses at the base of apartment buildings.



The balconies of this building are an integral part of the structure and do not seem “tacked” on. Outdoor terraces on the roof top provide additional amenity space for residents.



Building Roofs

Roofs of lower-scaled buildings are to utilize a variety of treatments and be designed to be attractive when viewed from above.

Towers are to contribute to the skyline, either through the incorporation of sculpted roof “caps”, terracing, or the sculpting of the upper floors of the building.

Vents, mechanical rooms and equipment, elevator penthouses, etc., are to be integrated into the architectural treatment of a building roof or screened with materials and finishes compatible with the building. Where appropriate, the provision of amenity space for occupants on building roofs should be provided.

Discussion:

- *Roof gardens, exterior deck landscaping, and other similar amenities should be considered for both the use of the building tenants and for those who see the building roof from adjacent buildings.*

The maximum height of parapets for all new buildings and developments are not to exceed 1.5 m to avoid creating false façades.

Signage

Signs are to be designed to complement the architecture of the building and not overwhelm the façade.

Billboards, large temporary signs, roof signs, revolving signs, and large standalone signs are not permitted as they do not respond to the pedestrian scale.

Types of signs that are appropriate within the Downtown area include:

- banners;
- fascia signs;
- awning signs;
- canopy signs;
- window signs; and
- projecting signs.

Signs that are externally lit are to have fixtures that are appropriately scaled to the building and signage. Over-illumination of signage is to be avoided. Buildings on corner lots are to have signs that address both the abutting street and the avenue.



Banners - This banner, made of a non-rigid material such as cloth, responds well to the pedestrian environment. Banner signs suspended on poles are recommended to signify Precincts within the Downtown area.



Fascia signs - This fascia sign, designed to fit in with the façade of the building, enhances its character. Fascia signs are traditionally used as storefront signage, creating a horizontal variation between the ground floor and upper floors.



Awning signs - Non-rigid roof-like awnings are used to identify the store as well as provide weather protection. Signage graphics are usually placed above the entrance.



Canopy signs - The sign is attached to the canopy that is in turn affixed to the building surface.



Window signs - These window signs are placed on the inside of the glass of a storefront window and door as secondary signage to the establishment.



Projecting signs - Smaller projecting signage, with a maximum projection of 1.0 m, allows for easy retail/business identification on a pedestrian scale.



Lighting

Building exteriors are to be lit to highlight their architectural and sculptural characteristics. Over-illumination of building signage is to be avoided.

Lighting is to be installed on the buildings and incorporated into any landscaped setbacks (in the form of wall sconces, garden lights, or set into edging features, etc.) to provide soft lower level illumination for the sidewalk areas as well as the building itself.

The use of free standing pole lighting is to be limited to entries and gathering places.



5.2.7 Parking Organization and Service Access/Loading

Vehicular Site Access

All vehicular access to parking and on-site passenger facilities and loading and service areas is to be from the lane to minimize disruptions to the surrounding sidewalks, for both residential and commercial uses and servicing.

Discussion:

- *Access to the back lanes is to be off of north-south streets, not east-west avenues.*
- *Locate any waiting or pick-up/drop-off areas internally to the site, not in the public right-of-way or in a required setback.*

Access to parking is to be via the back lanes and the north/south streets.





Vehicular entrances are to be consolidated to serve multiple buildings within each block in order to minimize the number of interruptions in the street wall and to reduce the number of potential conflicts with pedestrians and cyclists.



Vehicular entrances are to be consolidated to serve multiple buildings within each block.

Vehicular entrances designed as portals or archways which do not unduly break up the continuity of the street wall are encouraged.

The vehicular entrance to this underground parking garage is designed as an archway which does not break up the continuity of the street wall.



Surface Parking Lots

All surface parking lots, accessory and non-accessory, are to be paved to contribute to a cleaner and better looking Downtown.

Parking lot organization is to balance pedestrian and vehicle use.

Discussion:

- *Accessory and non-accessory parking lots are to be both pedestrian and vehicle-oriented. Amenities are to include generous landscaping and lighting at appropriate levels for personal and property safety. Every parking lot is to be considered not only for its function, but also for its look, and should be developed with the image of a park overlaying the vehicular parking.*

Clear pedestrian paths are to be developed through parking lots to destinations both on and off-site. Sidewalks within the site should connect with public sidewalks at the site perimeter, enhancing safety and visibility.

Surface parking lots are to be landscaped at the street edge to minimize their visual impact and to improve the pedestrian environment.

Discussion:

- *Landscaping is to define the city sidewalk and create a park-like treatment of the site.*
- *Street edges of parking areas are to contribute to the visual continuity of the street. This can be achieved with plantings, low open fences, colonnades, columns, and lampposts.*



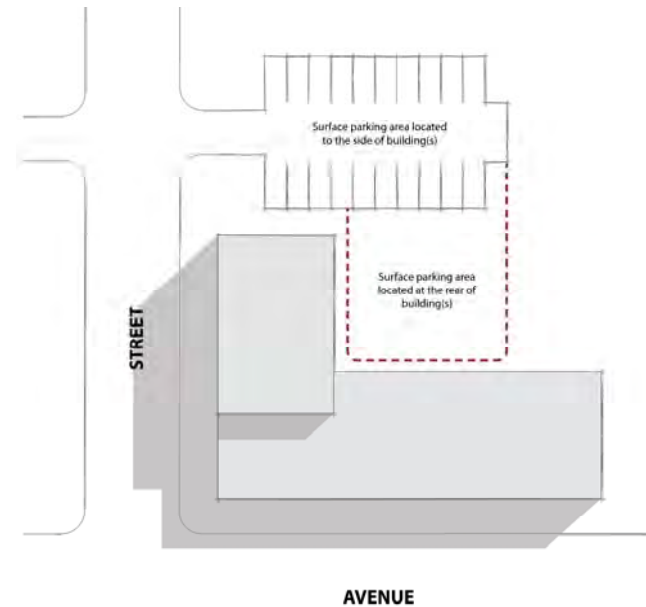
- *Landscaping is to include amenities such as shade trees, trellises, gateways, benches, and waste receptacles at street corners, and, where no structure exists, entrances to the site, transit stops, and other points of pedestrian activity.*
- *The design and character of screening of parking areas is to fit in with the existing landscape and built character of the street.*
- *Fences are to be 0.7 m to 1.0 m high.*
- *Street edge treatments are to be integrated with the abutting sidewalk edge design to ensure a continuity of paving.*
- *Pedestrian-scale lighting will identify pedestrian routes and site entrances.*
- *To ensure parking lot planting thrives, sound installation details (including irrigation) as well as a schedule for maintaining the upkeep and health of plants and soils are required.*



The street edges of parking areas are landscaped to minimize their visual impact.

Long term surface parking areas are to be located at the rear of buildings to ensure the sidewalks/pathways and building façades effectively define the street edge. In situations where it is impossible to accommodate surface parking areas behind buildings, parking areas may be provided along the side(s) of buildings. In both scenarios, the parking areas shall be appropriately screened from view.

Buildings on corner lots are not to locate surface parking lots and auto-related uses (service stations, auto sales, drive-through facilities, car washes, etc.) facing onto or visible from the adjacent street corner/intersection and pedestrian routes.



Surface parking areas are to be located at the rear of or beside buildings.



Underground/Above-ground Enclosed Parking Lots

Parking structures are to locate vehicle entrance/exit access points on the least busy of the surrounding streets, in order to minimize curb cuts and reduce conflicts with pedestrians. Vehicular entrances shall not be located at a view terminus or end of a T-intersection. Pedestrian access to all new parking structures is to be clearly demarcated, highly visible, and incorporated into the overall design of the building and/or development.

Negative impacts of parking ramps and vehicular entrances are to be minimized through treatments such as enclosure, screening, high quality finishes, sensitive lighting, and landscaping.



The parking ramp and entrance into the underground parking garage of this building is set back from the sidewalk and screened from view.

Storage and Service Areas

Loading areas and waste set-out areas are to be located at the rear of buildings.

Waste storage areas within buildings are encouraged, and if not feasible, waste should only be placed outside just prior to designated pick-up times.

Waste storage areas located outside are to be set back at least 1.0 m from an adjoining property line abutting low density residential areas, and shall be screened from view of adjacent properties.

Designated areas for storage, temporary truck parking, waste collection and compaction, and loading are to have a minimum separation of 7.5 m from any public street, sidewalks, and pedestrian spaces, and a minimum separation of 25.0 m from any adjacent residential properties.

Service areas are to be screened from view from the street by architecturally-compatible solid walls or landscaping treatments. Acceptable approaches could include landscape berms, brick masonry walls, cedar or painted wood fences, or trellises, ideally in combination with planting initiatives.



The waste storage area is partially enclosed and is architecturally treated with similar material and fencing as found elsewhere on the site.



Sustainability

Sustainable Building Design

New public buildings are encouraged to be Leadership in Energy and Environmental Design (LEED®) certified. New private buildings will also be encouraged to achieve LEED® certification.

Where possible, building materials are to be locally acquired.

Buildings are to be energy-efficient, through design considerations such as building orientation, high R-rating/low-embodied energy building materials, and energy-efficient appliances.

Buildings are to be designed to support a requirement of an extensive rooftop garden assembly covering at least 50% of the roof surface area to increase rainwater permeability and filtration, while reducing energy consumption and the urban heat island effect.

A high degree of indoor environmental quality should be achieved through design techniques including day lighting and the use of low-emission finishes formulated to low or zero volatile organic compounds (VOC) standards.

Building flexibility should be maximized to satisfy the varied demands of current and future users and residents.



A rooftop garden increases rainwater permeability and filtration, while reducing energy consumption and the urban heat island effect.

Sustainable Site Design

The site plan of a development should address environmental sustainability principles.

Discussion:

- *Water quality, consumption, and runoff are key site sustainability issues. A range of appropriate design measures should be considered such as the preservation of natural features, reduction of hard surfaces, and the addition of extensive landscaping.*
- *Landscape design should incorporate a wide range of strategies to minimize water consumption, e.g. through the use native species, the use of mulches and compost, alternatives to grass, and rainwater collection systems.*



- *Landscape plans should use deciduous street trees and on-site trees where these trees will grow to provide shade and help reduce temperatures inside adjacent units during the warmer months, and shed their leaves to allow sunlight and better heat penetration during cooler months.*

Existing significant trees, tree stands, and vegetation should be protected and incorporated into site design and landscaping.

Native plant materials should be used wherever possible.

The width of all planting beds should be at least 2.5 m wide to enable plant material to be massed to create a healthy and sustainable landscape.



A sustainable site design should be considered in new developments.

The distribution of outdoor lighting should be controlled through the use of full cut-off lighting fixtures to minimize light pollution and maintain a dark, night sky.

Parking areas should drain into vegetative or grassy swales that are incorporated into large common landscaping areas within project or perimeter landscaping.

Bioswales should be created next to parking lots and walkways to collect stormwater runoff to minimize the dependency on stormwater sewers. Bioswales should be planted with salt-tolerant shrubs and grasses to filter water before it percolates into the ground. They should be graded to direct water away from paved areas.

Drainage basins should be located throughout parking lots to collect stormwater. These basins should be planted with native plant materials that thrive in wet conditions.



5.3 STREET AND SIDEWALK ENVIRONMENT



5.3.1 Street Design

Provide coherent street walls, street trees, and other elements which give enclosure to the street space.

Allow for on-street parking.

Encourage low traffic speeds through narrower street widths, street bulbouts, and change in paving material at pedestrian crossings with streets.

Provide paving surfaces, catch basins, grates, etc. that are not hazardous to pedestrians or cyclists.

Provide tree canopies over sidewalks for summer shade.

In an effort to reduce pedestrian-vehicular conflict, curb cuts and vehicular access points associated with new and existing developments, shall be consolidated wherever possible and at a frequency not exceeding one driveway every 30.0 m.

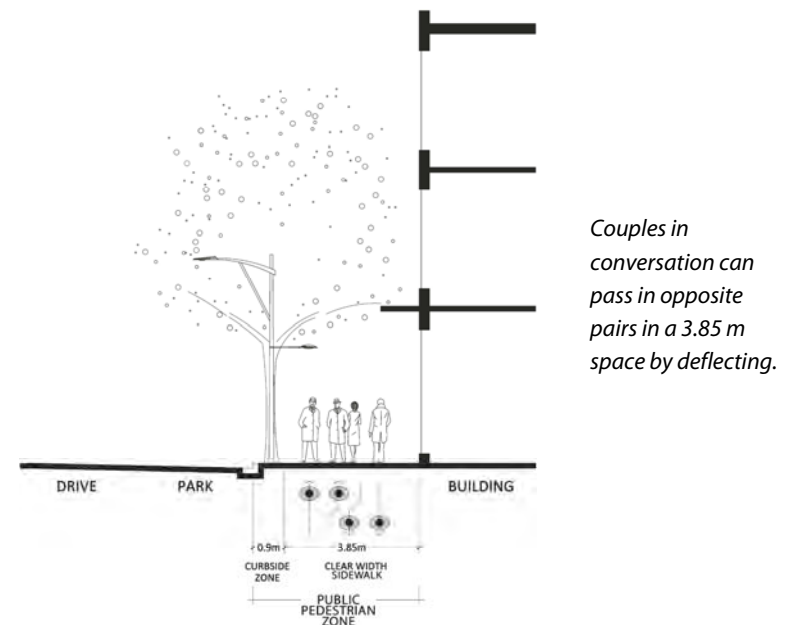
5.3.2 Sidewalk Design

Where applicable, sidewalks in the Downtown are to be a minimum clear unobstructed width of 3.85 m.

Discussion:

- *Sidewalks should have a sufficient clear width, free of obstruction, which would accommodate at least four people walking in opposite directions. Generous sidewalk widths provide a more inviting pedestrian environment.*

Additional sidewalk width may be gained through building setback lines.





Commercial Streets

In commercial areas, the curb-side zone, a minimum of 0.9 m wide, would accommodate elements which otherwise may obstruct pedestrian movement, including light poles, street tree planting, benches, parking meters, bicycle racks, newspaper boxes, post boxes, and litter receptacles. This strip would also be used to off-load deliveries and temporarily store snow.

Residential Streets

In residential areas, the sidewalk should be separated from the roadway by a minimum 1.5 m wide boulevard that is landscaped and tree lined.

A consistent sidewalk paving treatment is sought throughout the Downtown, with a visually-pleasing pattern and scoring, clearly demarcating the public realm boundary from the treatment of private plazas, and other private open spaces of adjoining property development.

Setback areas, including usable outdoor spaces, should continue the public sidewalk paving materials and finishing.

Special feature paving (including additional granite, different scoring patterns, brass inserts, or materials from the building) may be located in the setback areas but only at building entries.

Discussion:

- *Sidewalk paving is to achieve a number of objectives:*
 - *Create an attractive physical character at an appropriate pedestrian scale;*
 - *Create visual continuity for pedestrians throughout the Downtown; and*
 - *Provide high quality sidewalk paving, and minimize longer term maintenance costs consistent with achieving the desired pedestrian amenity.*



On-street parking, tree canopies, and wide sidewalks with a variety of amenities are all part of making an enjoyable and comfortable Downtown experience.



5.3.3 Street Trees and Furnishings

Street Trees

The street trees are to:

- have reasonable root containment habits;
- attain and maintain, in street conditions, a mature size of 14.0 m in height with an 8.0 - 9.0 m branch spread;
- be capable, with pruning, of maintaining a 2.0 m minimum clearance from sidewalk level to the underside of branches;
- have longevity, be of a species less prone to disease, and be clean and generally easy to maintain;
- be planted, in soil trenches a minimum of 1.0 m deep and 1.2 m wide, below grade to allow root systems space to expand, even in confined urban areas;
- have a minimum setback of 800 mm from the edge of the curb to the centre of the tree; and
- have a branch structure to provide a strong winter presence.

Street Trees in Tree Grates

A metal tree grate that can potentially be customized for a specific neighbourhood identity should be considered.



Street tree grates can potentially be customized for a particular neighbourhood identity.



Tree Spacing

A curbside row of trees is to be planted adjacent to the street at intervals of 6.0 - 8.0 m.

Where feasible, a second row of trees, planted at 6.0 - 8.0 m intervals, is to be staggered in relation to the curbside row of trees. The total number of trees is to be maximized.

Building entrances and major window bays are to be designed with tree spacing in mind.



Curbside street trees are planted at 6.0 m - 8.0 m spacings.

Street Furnishings

Newspaper and post office boxes and waste/recycling containers are to be grouped and located near pedestrian activity areas such as intersections, bus stops, open spaces, and entrances to both commercial and residential developments.

Seating is to be provided along streets, especially at street corners and incorporated into publicly-accessible developments.

Discussion:

- *The provision of comfortable and properly-located seating is fundamental to the popularity of urban open spaces.*
- *Seating should allow for a variety of social possibilities, including seating for small groups, pairs or individuals. It should be incorporated into both public rights-of-way and publicly accessible developments, especially building edges along the street.*
- *In addition to conventional benches, seating may be provided along the ledge of planters and stairs, or by individual movable chairs.*



The provision of comfortable and properly located seating is fundamental to the popularity of urban open spaces.



Street Lighting

Streets are to be lit utilizing only full cut-off lighting.

Discussion:

- *Full Cut-off (FCO) fixtures (also called fully shielded fixtures) prevent light from shining beyond the immediate area and up into the sky. Since no light shines horizontally, the amount of glare at a distance is significantly reduced from that of semi-cutoff or unshielded luminaires, thus increasing visibility. By reducing glare, our eyes can become more accustomed to the darkness, allowing us to see into areas with lower illumination levels farther from the fixture. The improvement in visibility with FCO fixtures permits the use of lower wattage bulbs that in turn reduces energy consumption. By lowering the illumination levels, less light will reflect off the ground into the surrounding trees, and into the sky. This also reduces the extent, and impact, of the artificial light on the night environment.*

Lighting is to focus on pedestrian areas and features including trees and public art. Pedestrian-scale, glare-free lighting is to be continuously provided along all streets.

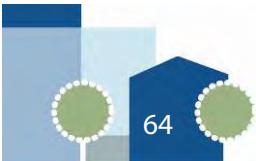
Street lighting is to have two scales: roadway lighting to light the road and pedestrian scale lighting to light the sidewalk.

A universal set of lamp poles, unique in its design, is to be developed for the Downtown for both sidewalk and roadway environments. It should incorporate components which would be unique to each of the neighbourhoods, and so would offer harmonious, uniform, and coherent streetscapes.

6.0 DEVELOPMENT CONCEPT

The proposed Development Concept for the Downtown area is shown on Figure 10 - Development Concept. The land use designations shown are a generalized concept for future land use opportunities in the area, and are based on input received from the Downtown Redevelopment Advisory Committee, City Administration, and the public consultation process.

The Development Concept addresses the overall policy direction contained in Section 4.0 of the Plan. More detailed policy direction and design guidelines are provided for each Precinct in Sections 7.0 - 13.0 of the Plan.







7.0 CORE COMMERCIAL PRECINCT

7.1 CONTEXT

The Core Commercial Precinct is located in the centre of the Plan area, and is bounded by 100 Avenue to the north, 104 Street to the east, 99 Avenue to the south, and 101 Street to the west. Surrounding development includes the Civic Precinct to the north, the Northeast Transition Precinct to the northeast, the Mall Redevelopment Precinct to the east, the Live/Work Precinct to the south, and the Old Fort/Open Space Precinct to the west (see Figure 7 - Precincts).

7.2 CHARACTER

This Precinct represents the heart of Fort Saskatchewan's downtown area, and is the oldest commercial area in the City. The majority of the uses in the area are commercial in nature, including: office, retail, service, or food establishments, with the exception of some medium density residential development along the east side of 101 Street, and on the west side of 104 Street, south of 100 Avenue. A public pool and a small park are located on the west side of 104 Street, north of 99 Avenue (see Figure 5 - Existing Uses).

There are some surface parking lots located throughout the Precinct, in addition to a small number of vacant lots. Overall, the majority of the area is occupied by a range of commercial, office, retail, and service uses, in buildings ranging from one to three storeys in height. Certain buildings are in poor physical condition, while others are nearing a point where some improvements would be beneficial. Most of the buildings in the area are well-kept, and are in good condition. There are few vehicle-oriented uses (drive-throughs, gas stations) in the area, helping to maintain a pedestrian-oriented environment. On-street angle parking is provided on 102 Street, 103 Street, and 104 Street, as well as along the north side of 99 Avenue. Sidewalk improvements along 102 Street and 103 Street have been recently completed, and boulevard trees line the north side of 99 Avenue, utilizing the former CN Rail right-of-way.



The Core Commercial Precinct is the established central area of the Downtown, and is characterized by a variety of retail, office, service, and eating/drinking establishments.



7.3 ISSUES AND OPPORTUNITIES

While the Core Commercial Precinct has been reasonably stable over time, there are some issues and opportunities that can be identified that can assist in helping the area to become the thriving business core of the Downtown.

Underutilization of land - while limited in number, there are some parcels in the Precinct that are either vacant or used only for surface parking. Further, current zoning in the area provides the opportunity for commercial development up to approximately four storeys in height (or approximately eight storeys under certain circumstances for mixed-use developments), yet few buildings in the area take advantage of this opportunity. An increase in the intensification of development in this area will help it to retain and increase its vitality.

Limited commercial services available - despite the fact that the Precinct has a number of commercial uses, the range of retail, service, and eating and drinking establishments is somewhat limited. Encouraging a broader range of uses that residents and visitors to the area would find desirable, both during the day and in the evening, would assist in making the Precinct a destination point for the community.

Maintenance and appearance of existing properties - a key element in creating and maintaining a thriving commercial core is ensuring that properties and buildings in the area provide an attractive and inviting environment for patrons and visitors alike. While many business storefronts in the Precinct are well-maintained, there are a number of buildings or storefronts that are falling into disrepair. Poor property and building maintenance shows a lack of pride of ownership, and over time, will lead to a decline in interest for new investment in the area, and in customers taking their business elsewhere.

Opportunities for mixed-use development - the Core Commercial Precinct has been, and will continue to be, the business centre of the City. However, creating an environment that fosters a mix of commercial and residential development can assist greatly in ensuring an area is vibrant both day and night. Current zoning in the area provides for residential uses to be developed over the ground floor of a building. This opportunity should be encouraged, where possible, for new developments.

Promotion of the historic character of the area - despite its being the oldest commercial area in the City, the stock of historic buildings in this Precinct is not significant. The most notable building in the area is the Masonic Lodge on 101 Street, which is currently used for a personal services business. However, this area offers a tremendous opportunity to provide a unique character that reflects back on its former prominence as the business and historical centre of the community.



Inconsistent streetscape elements - it has been noted that there has not been a comprehensive vision identified for public infrastructure in the Downtown area. As a result, there is a variety of styles of streetlights, planters, and other street furniture that do not contribute to a unified appearance for the area. There is also a lack of trees and other vegetation that can help make the area more attractive and inviting. To assist in making the Downtown a desirable area to invest in, a consistent and comprehensive approach to public realm improvements that will uniquely identify the area is needed.

Sustainable development - in conjunction with the broader goals of the Fort Saskatchewan Sustainability Plan, opportunities exist for new development in the area to achieve sustainable building and development principles.

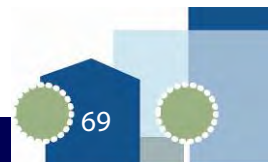
7.4 GENERAL INTENT OF THE PRECINCT

The general intent of the Core Commercial precinct is to retain and enhance its role as the key commercial and business centre of the City. Development in the area, particularly at street-level, will be focused on retail, office, eating and drinking establishments, and service uses, with opportunities for residential development being provided only above the ground floor of buildings (see Figure 10 - Development Concept).

7.5 POLICIES

7.5.1 Land Use

- 1) The primary focus for the Precinct is to accommodate commercial development. In particular, retail, office, personal service, and eating/drinking establishments will be encouraged.
- 2) Encourage a variety of commercial and retail uses that are oriented to the street to make the area more attractive to pedestrians.
- 3) New residential development that does not provide commercial uses on the ground floor will not be permitted.
- 4) Mixed-use development is permitted, provided the ground floor of the building accommodates commercial development.
- 5) The maximum building height in this Precinct shall be 4 storeys (14.0 m). Residential buildings with commercial uses on the ground floor shall have a maximum height of 8 storeys (25.0 m), at the discretion of the Development Authority.
- 6) Commercial development on the ground floor of buildings will have entrances that address the street and the pedestrian through the use of glazing, weather protection, signage, and lighting.





- 7) Support the provision of affordable housing units in residential developments in this Precinct in conjunction with applicable provincial and federal programs.
- 8) The area should cater to both daytime and nighttime activities, in all seasons.
- 9) Buildings should be built to the front property line, or to a building setback line as established in Section 7.6. A minimal setback may be permitted in order to accommodate active space associated with the related ground floor commercial use, such as patios or plazas.
- 10) The façades of buildings on corner sites must address both the abutting street and avenue.
- 11) Developments should adhere to Crime Prevention Through Environmental Design (CPTED) principles in order to make the area inviting for residents, employees, and visitors.
- 12) Abrupt and excessive differences in building scale and massing should be minimized.
- 13) New vehicle-oriented uses, such as drive-throughs, will be discouraged in the area.

7.5.2 Circulation

- 1) 100 Avenue between 101 Street and 104 Street will undergo an overall rehabilitation program in 2010. As part of this project, streetscaping and boulevard treatment improvements will be incorporated that assist in achieving aspects of the design guidelines for this area. The City will work with property owners on 100 Avenue to allow opportunities for input on the design and comments on specific proposed improvements.
- 2) Streetscape improvements that are consistent with the concept for 100 Avenue should be considered for 102 Street, 103 Street, and 104 Street.
- 3) 99 Avenue, 100 Avenue, and 101 Street will continue to serve as the major access points into the Downtown. 99 Avenue and 101 Street will continue to function at an arterial roadway standard, while 100 Avenue will function at a collector roadway standard.
- 4) Enhance pedestrian and bicycle connections from the Core Commercial Precinct to the Old Fort/Open Space Precinct to ensure ease of movement between these two areas.
- 5) Consider the establishment of a transit hub that provides commuter access to other areas in the Edmonton Capital Region in this Precinct.



7.5.3 Parking

- 1) Surface parking areas in the front yard of developments between the sidewalk and a building will be discouraged. On-site parking should be provided at the rear of buildings or underground.
- 2) Existing on-street angle and parallel parking will remain to ensure an adequate supply of vehicular parking for those working in and visiting the area.
- 3) Consider proposals for the reduction of required on-site parking for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct, in accordance with the parameters outlined in Section 2.33 of the Land Use Bylaw.
- 4) Consider the provision of accessible and secure bicycle parking facilities for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct.
- 5) As part of the redevelopment of a site, require any existing surface parking areas to be improved to comply with the standards of the Land Use Bylaw, in terms of hardsurfacing and landscaping, if these areas are not currently adhering to those standards.

7.5.4 Open Space

- 1) Explore opportunities to acquire land or partner with existing landowners to develop small pocket parks fronting onto 102 Street and 103 Street, in order to provide open space opportunities in the area.
- 2) Consider the installation of entrance features on 99 Avenue where it intersects with 101 Street and 108 Street to bring attention to the fact that visitors are entering the Downtown.

7.5.5 Social/Cultural

- 1) Existing historic resources should be protected and enhanced, through adaptive re-use.
- 2) Encourage public art installations and/or building murals in this Precinct to add vitality and interest to the public realm.
- 3) Consider the development of a municipal art gallery in this Precinct.
- 4) Establish a Summer Storefront RCMP in this Precinct.
- 5) Establish a Public Art Program that would require 1% of the constructions costs of new developments in this Precinct to be provided towards the purchase and installation of public art on a site.





7.5.6 Sustainability

- 1) Encourage new public buildings and facilities to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 2) Encourage new private developments to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 3) Explore opportunities to connect structures in the area to an alternative energy supply system, such as a district heating/cooling or geothermal system.
- 4) Conduct energy-efficiency audits on any City-owned buildings in the area and address identified deficiencies.
- 5) Consider capital projects in the area that meet the criteria of the Federation of Canadian Municipalities' Green Municipal Fund.
- 6) Encourage owners of commercial buildings in the area to apply for assistance in making energy-efficiency improvements to their buildings through the federal ecoENERGY Retrofit Program.

7.5.7 Economic Development

- 1) Explore the development of a Façade Improvement Program that provides municipal grants to business/property owners that undertake rehabilitation or improvements to the exterior façades of their buildings in this Precinct.
- 2) Encourage types of businesses not presently situated in the Downtown to locate in this Precinct.
- 3) Consider a reduction in the property tax assessment for businesses that are located in this Precinct.
- 4) Consider a cash-in-lieu payment for developments that propose a reduction in on-site parking in this Precinct.

7.6 DESIGN GUIDELINES

7.6.1 Background

The Core Commercial Precinct is to be primarily commercial in nature with the opportunity of residential uses above a commercial retail/office base. A strong commercial retail base is to be established for the area, encouraging pedestrian activity in all seasons within the core of Downtown.



7.6.2 Buildings and Sites

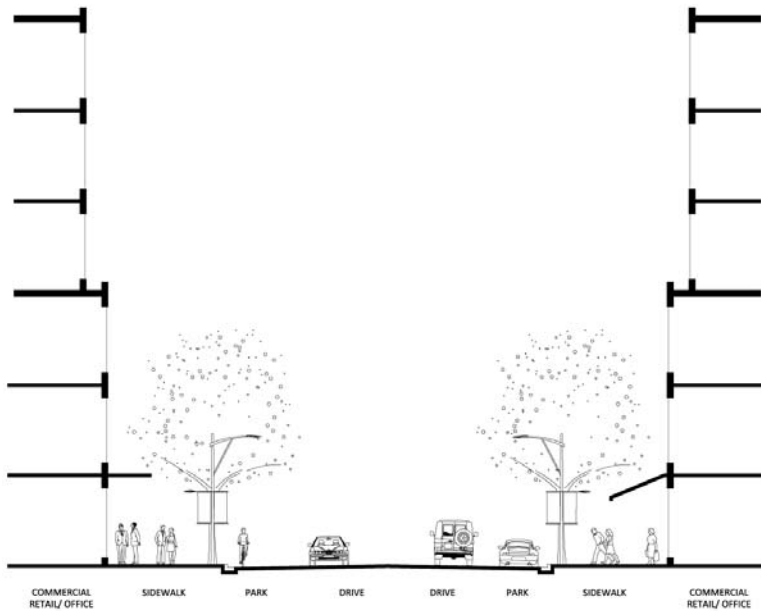
- 1) The Core Commercial Precinct will establish a range of commercial retail/office buildings and mixed-use buildings. New buildings that are exclusively residential in nature will not be allowed within this Precinct.
- 2) A building setback of a minimum of 2.0 m, measured from the front or side (for corner parcels) property line, may be required to provide space for a continuous public pedestrian zone of 5.5 m.
- 3) Create waiting areas under canopies for weather protection at the ground floor of buildings and on street corners, with minimum dimensions of 1.8 m x 1.8 m.
- 4) Continuous weather protection is required at the ground level for buildings on 99 Avenue and 100 Avenue, in the form of awnings or canopies. These are to be of sufficient depth (minimum 1.8 m) to shelter outdoor display and seating areas, as well as to protect walking space on the adjacent public sidewalk.

7.6.3 Street and Sidewalk Environment

- 1) A continuous public pedestrian zone, a minimum of 5.5 m in width, measured from the front of the adjacent curb to the front face of the building on the ground floor, is to be provided for sites in this Precinct.

Discussion:

- *The 5.5 m public pedestrian zone is comprised of a 0.9 m wide curbside zone for the placement of sidewalk furnishings, and a 4.6 m wide sidewalk.*
 - *To achieve a continuous 5.5 m public pedestrian zone, a building setback of a minimum of 2.0 m, measured from the front or side (for corner parcels) property line of a site, may be necessary.*
 - *The entire public pedestrian zone is to be hard-surfaced with a consistent surface pavement treatment from the City sidewalk to the building face.*
- 2) The streets within this Precinct will be lined with street trees planted with tree grates at intervals of 6.0 - 8.0 m.
 - 3) Streetscape elements such as banner flags, lighting, and hanging planters are to be coordinated along 99 Avenue and 100 Avenue. The streetscape elements will help distinguish the two "Main Streets" apart from one another and to create an interesting pedestrian environment.



A street section illustrating the potential street and sidewalk environment within the Core Commercial Precinct.



Buildings within the Core Commercial Precinct should be appropriately scaled to surrounding buildings and architecturally-treated to address the street that they front. Elements such as large floor-to-ceiling glazing, canopies, use of richer materials, and outdoor patio space should be considered to create an interesting and active pedestrian environment.



8.0 LIVE/WORK PRECINCT

8.1 CONTEXT

The Live/Work Precinct is located in the southwest portion of the Plan area, and is bounded by 99 Avenue to the north, 104 Street to the east, 98 Avenue to the south, and 101 Street to the west. Surrounding development includes the Core Commercial Precinct to the north, the Mall Redevelopment Precinct to the east, existing low density residential uses to the south, and school and park uses to the west (see Figure 7 - Precincts).

8.2 CHARACTER

This Precinct provides one of the more eclectic areas of development in the Downtown. Overall, a mix of residential and commercial uses occurs in this area. Commercial uses range from retail and office to professional and personal service uses, primarily located along the east side of 101 Street and the south side of 99 Avenue. Residential uses range widely, from older single detached dwellings (primarily on the west side of 102 Street), to newer duplexes (on the east side of 102 Street and the west side of 103 Street), to newer townhouses (on the north side of 98 Avenue, between 102 Street and 103 Street), to older low-rise apartments in various locations. Several of the buildings in this area are nearing a point where some improvements would be beneficial; however, few are in poor condition.

There is a larger surface parking lot on the west side of 103 Street, smaller parking lots associated with business uses, and angle and parallel parking is provided on the streets in the area. A few undeveloped parcels are also present in this Precinct. The commercial uses along 99 Avenue are primarily vehicle-oriented in nature, including two gas stations and a convenience store, and serve the traffic flowing along this arterial roadway (see Figure 5 - Existing Uses).



The Live/Work Precinct is comprised of an interesting mix of retail, office, and low and medium density residential uses.



8.3 ISSUES AND OPPORTUNITIES

The Live/Work Precinct is in a state of flux at the present time, with newer, suburban-style residential uses being developed in close proximity with higher density residential and established commercial uses. There are some issues and opportunities in this area that can assist in re-focusing the direction for this portion of the Downtown.

Limited range of commercial services available - while this Precinct does have a range of commercial uses, the range of retail, service, and eating and drinking establishments is somewhat limited, similar to the Core Commercial Precinct. Encouraging a broader range of uses that residents, visitors, and those that work in the area would find desirable, both during the day and in the evening, would assist in making this Precinct a more vibrant component of the Downtown.

Maintenance and appearance of existing properties - a key element in creating and maintaining thriving neighbourhoods in a Downtown area is ensuring that properties and buildings provide an attractive and inviting environment for visitors and residents. While many of the properties and buildings in this Precinct are well-maintained, there are some that have fallen into disrepair, or are in need of some improvements. Poor property and building maintenance shows a lack of pride of ownership, and over time, will lead to a decline in interest for new investment in the area.

Underutilization of land - there are a few parcels in this Precinct that are used for surface parking or are simply vacant. Further, some newer developments have been approved in the area that resulted in low-density, suburban-style residential uses that are not generally compatible with a Downtown location. Much of the current zoning in the area provides for commercial development up to approximately four storeys in height (or approximately eight storeys under certain circumstances for mixed-use developments), yet few buildings in the area take advantage of this opportunity. An increase in the intensification of development in certain portions of this Precinct is warranted given its proximity to the Downtown core; however, consideration should be made to limiting mixed-use developments to a maximum height of four storeys to ensure a better transition to the existing low density residential uses to the south of 98 Avenue. Future residential development in the area should be limited to townhouse or low-rise apartment uses up to four storeys in height.

Opportunities for mixed-use development - building upon the area's true mixed-use form, opportunities exist to encourage both new commercial and residential development in the area. This Precinct can act as a transition from the core commercial area north of 99 Avenue to the low-density residential area to the south of 98 Avenue.



Transition to low-density residential uses to the south - while this Precinct has the opportunity to provide mixed-use development at higher levels of density that presently exist, care must be taken to ensure a proper transition of the scale of development to the existing low-density residential neighbourhood on the south side of 98 Avenue. A focus on lower-scale, medium-density residential uses along the north side of 98 Avenue would assist in maintaining a compatible mix of development along both sides of this roadway.

Better linkages to the Downtown from the south - while an appropriate transition of development along 98 Avenue is important, so too are connections between the two areas. The 98 Avenue roadway and associated service road on the south side of the street act somewhat as a barrier to pedestrian traffic moving northwards into the Downtown core. Better pedestrian connections across 98 Avenue into the Live/Work Precinct would help promote more interaction between the two neighbourhoods.

Need for streetscape improvements - much of the existing streetscaping in the area is in need of improvement. There is also a lack of trees and other vegetation that can help make the area more attractive and inviting. New streetscaping concepts and improvements developed for the Core Commercial Precinct should be extended into this area to create a more consistent and comprehensive approach to public realm improvements.

Sustainable development - in conjunction with the broader goals of the Fort Saskatchewan Sustainability Plan, opportunities exist for new development in the area to achieve sustainable building and development principles.

8.4 GENERAL INTENT OF THE PRECINCT

The general intent of the Live/Work Precinct is to promote the area as a true mixed-use component of the Downtown. New retail, office, and service uses will be encouraged into the area, primarily at street-level, but also on other levels of buildings. Medium-density residential development will also be encouraged, and street-level units will also be accommodated. Innovative opportunities for live/work arrangements will be actively supported, such as artists' galleries and living spaces. Opportunities for new low-density, suburban-style residential development will no longer be accommodated in the area (see Figure 10 - Development Concept).

8.5 POLICIES

8.5.1 Land Use

- 1) The primary focus is to support both commercial and residential development in the area, with particular emphasis on live/work opportunities.
- 2) Office, cultural (e.g. art gallery), retail, service, and medium density residential uses will be actively encouraged.





- 3) Street-level floor space in buildings may be either residential or commercial in nature, with any combination of uses above.
 - 4) New standalone low density residential development, such as single detached and semi-detached dwellings, will no longer be permitted in the area.
 - 5) The maximum building height in this Precinct shall be 4 storeys (14.0 m).
 - 6) Support the provision of affordable housing units in residential developments in this Precinct in conjunction with applicable provincial and federal programs.
 - 7) Commercial development on the ground floor of buildings will have entrances that address the street and the pedestrian through the use of glazing, weather protection, signage, and lighting.
 - 8) The principal entrance to dwelling unit(s) provided on the ground floor of buildings will provide direct external access to the adjacent public sidewalk.
 - 9) The area should cater to both daytime and nighttime activities, in all seasons.
 - 10) Buildings should be built to the front property line, or to a building setback line as established in Section 8.6. A minimal setback may be permitted in order to accommodate active space associated with related ground floor commercial use, such as patios or plazas, or with related ground floor residential use, such as patios or gardens.
 - 11) The façades of buildings on corner sites must address both the abutting street and avenue.
 - 12) Developments should adhere to Crime Prevention Through Environmental Design (CPTED) principles in order to make the area inviting for residents, employees, and visitors.
 - 13) New vehicle-oriented uses, such as drive-throughs, will be discouraged in the area.
 - 14) Consider the re-use of existing single detached dwelling buildings for non-residential uses such as offices, services, retail, or eating and drinking establishments.
- ### 8.5.2 Circulation
- 1) Streetscaping improvements considered for this area will be designed in such a way to assist in achieving aspects of the design guidelines for this Precinct.



- 2) 99 Avenue and 101 Street will continue to serve as major access points into the Downtown area. Both roadways will continue to function at an arterial roadway standard. 98 Avenue will provide alternate access into the Downtown for areas to the south, but will operate at a collector roadway standard.
- 3) Enhance pedestrian and bicycle connections from the low-density residential area to the south of 98 Avenue to the Live/Work Precinct to encourage better non-vehicular connections into the Downtown.

8.5.3 Parking

- 1) Surface parking areas in the front yard of developments between the sidewalk and a building will be discouraged. On-site parking should be provided at the rear of buildings or underground.
- 2) Existing on-street angle and parallel parking will remain to ensure an adequate supply of vehicular parking for those working in and visiting the area.
- 3) Consider proposals for the reduction of required on-site parking for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct, in accordance with the parameters outlined in Section 2.33 of the Land Use Bylaw.

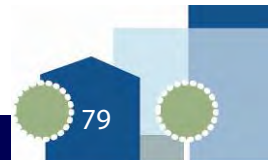
- 4) Consider the provision of accessible and secure bicycle parking facilities for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct.
- 5) As part of the redevelopment of a site, require any existing surface parking areas to be improved to comply with the standards of the Land Use Bylaw, in terms of hardsurfacing and landscaping, if these areas are not currently adhering to those standards.

8.5.4 Open Space

- 1) Consider the installation of an entrance feature on 101 Street where it intersects with 99 Avenue to bring attention to the fact that visitors are entering the Downtown.

8.5.5 Social/Cultural

- 1) Existing historic resources should be protected and enhanced, through adaptive re-use.
- 2) Encourage public art installations and/or building murals in this Precinct to add vitality and interest to the public realm.
- 3) Establish a Public Art Program that would require 1% of the constructions costs of new developments in this Precinct to be provided towards the purchase and installation of public art on a site.





8.5.6 Sustainability

- 1) Encourage new public buildings and facilities to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 2) Encourage new private developments to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 3) Explore opportunities to connect structures in the area to an alternative energy supply system, such as a district heating/cooling or geothermal system.
- 4) Conduct energy-efficiency audits on any City-owned buildings in the area and address identified deficiencies.
- 5) Consider capital projects in the area that meet the criteria of the Federation of Canadian Municipalities' Green Municipal Fund.
- 6) Encourage owners of commercial buildings in the area to apply for assistance in making energy-efficiency improvements to their buildings through the federal ecoENERGY Retrofit Program.

8.5.7 Economic Development

- 1) Explore the development of a Façade Improvement Program that provides municipal grants to business/property owners that undertake rehabilitation or improvements to the exterior façades of their buildings in this Precinct.
- 2) Encourage types of businesses not presently situated in the Downtown to locate in this Precinct.
- 3) Consider a reduction in the property tax assessment for businesses that are located in this Precinct.
- 4) Consider a cash-in-lieu payment for developments that propose a reduction in on-site parking in this Precinct.

8.6 DESIGN GUIDELINES

8.6.1 Background

The Live/Work Precinct will establish a variety of mixed-use buildings with emphasis on integrated live/work units. The intent of the Precinct is geared towards specialized professionals/businesses, while accommodating opportunities for a variety of residential arrangements. Types of businesses that are encouraged in the area are art studios, specialty boutiques, design offices, and yoga studios.

An integrated live/work unit comprises a work space on the ground floor and a living space above which is occupied by a



single tenant. The integrated live/work unit is geared towards businesses that have no more than 2 non-resident employees.

8.6.2 Buildings and Sites

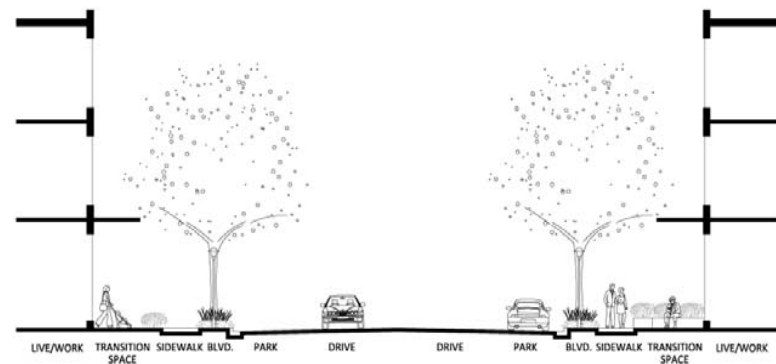
- 1) Buildings with designated office/retail units at the base will be located along 99 Avenue facing the street with continuous retail frontages that will not be interrupted by vehicular accesses, surface parking lots, or blank walls.
- 2) Buildings are to accommodate convertible space on the ground floor by providing a minimum ceiling height of 4.0 m and large commercial doorway(s).
- 3) Buildings may be set back a maximum of 3.0 m from the front property line to provide for a private transition space from the public pedestrian zone to integrated live/work units on the ground floor of buildings.
- 4) Front entrances are to be at grade and universally accessible.

8.6.3 Street and Sidewalk Environment

- 1) A continuous public pedestrian zone, a minimum of 3.5 m in width, measured from the front of the adjacent curb to the front face of the building on the ground floor, is to be provided for sites in this Precinct.

Discussion:

- The 3.5 m pedestrian zone is comprised of a 2.0 m wide landscaped boulevard, and a 1.5 m wide sidewalk.
- Where applicable, a building setback of a minimum of 1.0 m, measured from the front or side (for corner parcels) property line, may be required to achieve the continuous 3.5 m public pedestrian zone.
- The entire public pedestrian zone is to be hard-surfaced with a consistent surface pavement treatment from the City sidewalk to the building face.



A street section illustrating the potential street and sidewalk environment within the Live/Work Precinct.



- 2) The sidewalk environment within this Precinct should not have lawns within the building setback area and on the boulevards, adjacent to the sidewalk.

Discussion:

- *The boulevard will be planted with street trees and landscaped as rain gardens. The rain garden will feature a variety of moisture-tolerant native plants which will increase permeable surfaces as well as rainwater retention and filtration. This will create a distinct streetscape character.*



A landscaped boulevard that is not mowed lawn will create a distinct streetscape character for the Live/Work Precinct.



Live-work units on the ground floor of buildings should have floor-to-ceiling heights of 4.0 m and large commercial doorways to allow for easy conversion of space. Individual units are to have primary entrances that front the street and are universally accessible.



9.0 MALL REDEVELOPMENT PRECINCT

9.1 CONTEXT

The Mall Redevelopment Precinct is located in the southeastern portion of the Plan area. The main sub-area of the Precinct is bounded by 99 Avenue to the north, the ravine/park area east of 108A Street to the east, 98 Avenue to the south, and 104 Street to the west. The other sub-area of the Precinct is bounded by the lane south of 100 Avenue to the north, 106 Street to the east, 99 Avenue to the south, and 104 Street to the west. Surrounding development includes a portion of the Northeast Transition Precinct to the north, existing low density residential uses to the northeast, a ravine/park area to the east, existing low density residential uses to the south, and the Live/Work and Core Commercial Precincts to the west (see Figure 7 - Precincts).

9.2 CHARACTER

Overall, this Precinct represents the most significant redevelopment opportunity in the Downtown area. Two main sites dominate this Precinct: the existing Fort Mall, located south of 99 Avenue, between 104 Street and 108 Street; and the former Safeway site, located north of 99 Avenue, between 104 Street and 106 Street.

The Fort Mall site is approximately 4.9 ha in size, and is occupied by the Mall building, which is approximately 16,000 m² in size, and was built in 1975. Most of the Mall is currently unleased; however, there are a few tenants still occupying space. The building is surrounded on the north, east, and south sides by a significant amount of surface parking, most of which is in marginal condition. The exterior of the building is in poor condition, and certain areas are subject to vandalism and graffiti. The former Safeway site is approximately 2.4 ha in size, and is presently occupied by a Giant Tiger retail outlet in the former Safeway. Three other smaller buildings on the site are occupied by a variety of office, retail, service, and eating and drinking establishments. The bulk of the site is made up of a surface parking area fronting 99 Avenue and 104 Street. Neither of these two sites interacts well with the adjacent streets, and both are mainly oriented to vehicular traffic.



The Fort Mall development, looking east along 99 Avenue, is an underutilized site in the Downtown area.



In addition to the two mall sites, there is a blockface of development on the east side of 104 Street, between 98 Avenue and 99 Avenue. Parcels in this area are developed with a mix of single detached residences and a three storey office/retail complex at the southeast corner of 99 Avenue and 104 Street. The block between 98 Avenue and 99 Avenue, east of 108 Street, is developed with a mix of office/retail and residential uses. A four storey office building and associated surface parking lot is located at the southeast corner of 99 Avenue and 108 Street. A smaller, one storey retail/office building is located at the northwest corner of 98 Avenue and 108 Street, and is partially vacant. East of 108A Street are some newer townhomes and an older single detached dwelling, all of which front onto the adjacent ravine (see Figure 5 - Existing Uses).

9.3 ISSUES AND OPPORTUNITIES

The two mall sites in this Precinct represent the most significant redevelopment opportunity in the Downtown. How these sites redevelop will be critical in assisting the rest of the area to be enhanced, and ensuring that they create a harmonious transition with development in the surrounding areas. Issues and opportunities that can be identified include:

Underutilization of land - the two mall sites provide outdated, suburban-style retail outlets in a Downtown setting. They are both occupied by large, single-storey buildings, and are otherwise developed with large areas of surface parking. A tremendous opportunity exists to have these two sites redeveloped for a variety of medium density/medium rise residential or commercial uses. Opportunities also exist to provide high density/high rise residential or commercial uses on these sites, in particular locations, with site specific design parameters to ensure development is sensitive to the surrounding areas.

Transition and land use compatibility - the majority of these two sites are adjacent to established low density residential neighbourhoods. While opportunities exist for significant intensification of the two mall sites in this Precinct, it will be critical to ensure that any new development presents a harmonious transition to the adjacent areas. Development areas abutting 106 Street, 99 Avenue, 108 Street, and 98 Avenue will need to be developed at a lower density and scale to provide a transition from the low density areas to any new buildings on these sites.

Street-level development - given the large size of the two mall sites, the street-level development they provide will be an important component of better integrating them into the surrounding areas. Street-level areas on the two mall sites along 104 Street and 99 Avenue will focus more on providing commercial uses. Along 98 Avenue, 106 Street, and 108 Street, street-level uses on the two mall sites will focus more on residential uses.



Improved streetscaping elements - the north side of 99 Avenue, east of 104 Street, has been developed with an attractive walkway lined with boulevard trees. The south side of this roadway would benefit from a complementary streetscaping improvement, to make this portion of the Fort Mall site more attractive to pedestrians. Streetscaping elements developed for the Core Commercial and Live/Work Precincts should be extended onto 104 Street. Streetscaping upgrades along the north side of 98 Avenue should also be provided to soften the hard edge of the Fort Mall site.

Better linkages to the Downtown from the south - while an appropriate transition of development along 98 Avenue is important, so too are connections between the two areas. The 98 Avenue roadway and associated service road on the south side of the street act somewhat as a barrier to pedestrian traffic moving northwards into the Downtown core and the Fort Mall site. Better pedestrian connections across 98 Avenue into the Mall Redevelopment Precinct would help promote more connection between the two neighbourhoods.

Maintain residential nature of 108A Street - the newer townhomes along 108A Street provide a residential enclave adjacent to the ravine/park area. This residential component should be retained for any new development.

Open space opportunities - due to the large size of the two mall sites, an opportunity exists to provide significant open space areas as part of their redevelopment. Efforts should be made to ensure that portions of these open space areas are retained for public access and use, and be connected to pedestrian linkages provided to other adjacent areas.

Vehicular access and parking - to prevent shortcutting through the Fort Mall site, care needs to be taken to ensure that vehicular access to this site is offset from any existing accesses to the south. On-site parking on both of the mall sites should be provided underground. If this is not feasible, surface parking areas should be directed away from the adjacent streets.

Sustainable development - in conjunction with the broader goals of the Fort Saskatchewan Sustainability Plan, opportunities exist for new development in the area to achieve sustainable building and development principles.



9.4 GENERAL INTENT OF THE PRECINCT

The general intent of the Mall Redevelopment Precinct is to provide development that is focused on residential and commercial mixed-use, at a higher density and scale than is seen elsewhere in Fort Saskatchewan. This area represents an opportunity to significantly increase the residential population of the Downtown area, a key aspect in rejuvenation. High density/high rise buildings may be accommodated in specific locations, subject to design guidelines to ensure that they relate to the adjacent areas. Street-level uses will be oriented to the pedestrian, providing commercial and residential development in different locations. Open space and pedestrian connections will be provided to ensure ease of movement to and from the area, and opportunities for recreation (see Figure 10 - Development Concept).

9.5 POLICIES

9.5.1 Land Use

- 1) The primary focus for the area is to provide medium to high density and scale residential and commercial development on the two mall sites.
- 2) Commercial and residential mixed-use development at a medium density and scale will be provided along 104 Street, south of 99 Avenue.
- 3) Retain the east side of 108 Street for commercial/residential mixed-uses, with a maximum building height of 4 storeys (14.0 m).
- 4) Retain the area east of 108A Street for low and medium density/scale residential development.
- 5) Street-level floor space in buildings on the Fort Mall site will be oriented to commercial uses along 99 Avenue, and residential uses along 108 Street and 98 Avenue.
- 6) Street-level floor space in buildings on the former Safeway site will be oriented to commercial uses along 99 Avenue and 104 Street, and residential uses along 106 Street.
- 7) Commercial development on the ground floor of buildings will have entrances that address the street and the pedestrian through the use of glazing, weather protection, signage, and lighting.
- 8) The principal entrance to dwelling unit(s) provided on the ground floor of buildings will provide direct external access to the adjacent public sidewalk.
- 9) The height of high rise residential and commercial buildings shall be determined using the 45° angular plane method.
- 10) Medium rise residential and commercial buildings shall be limited to a maximum height of 8 storeys (25.0 m).

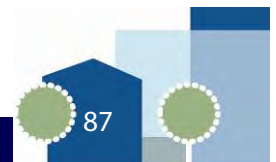


- 11) Low rise residential and commercial buildings shall be limited to a maximum height of 3 storeys (11.0 m).
- 12) Commercial uses on the south side of 99 Avenue shall be limited to a maximum height of 4 storeys (14.0 m).
- 13) High density residential buildings shall be limited to a maximum density of 250 units per hectare.
- 14) Medium density residential buildings shall be limited to a maximum density of 90 units per hectare.
- 15) Low density residential buildings shall be limited to a maximum density of 50 units per hectare.
- 16) Support the provision of affordable housing units in residential developments in this Precinct in conjunction with applicable provincial and federal programs.
- 17) The area should cater to both daytime and nighttime activities, in all seasons.
- 18) Buildings should be built to the front property line, or to a building setback line as established in Section 9.6. A minimal setback may be permitted in order to accommodate active space associated with the related ground floor commercial use, such as patios or plazas.
- 19) The façades of buildings on corner sites must address both the abutting street and avenue.

- 20) Developments should adhere to Crime Prevention Through Environmental Design (CPTED) principles in order to make the area inviting for residents, employees, and visitors.
- 21) New vehicle-oriented uses, such as drive-throughs, will be discouraged in the area.

9.5.2 Circulation

- 1) Streetscaping improvements considered for this area will be designed in such a way to assist in achieving aspects of the design guidelines for this Precinct.
- 2) 99 Avenue will continue to serve as a major access point into the Downtown area. This roadway will continue to function at an arterial roadway standard. 98 Avenue will provide alternate access into the Downtown for areas to the south, but will operate at a collector roadway standard.
- 3) Enhance pedestrian and bicycle connections from the low-density residential area to the south of 98 Avenue to the Mall Redevelopment Precinct to encourage better non-vehicular connections into the Downtown.
- 4) Ensure pedestrian connections on and through the Fort Mall site link to new connections on 98 Avenue and provide access to 99 Avenue.





- 5) Require vehicular accesses to the Fort Mall site to be off-set from existing roadways along 98 Avenue and 99 Avenue to reduce potential for short-cutting through the site.
- 6) Streetscape improvements that are consistent with the appearance of the north side of 99 Avenue should be considered for the south side of the avenue.
- 7) Streetscape improvements to the north side of 98 Avenue should be made to improve the appearance and walkability of this roadway.
- 8) Streetscape improvements that are consistent with the concept for the Core Commercial and Live/Work Precincts should be considered for 104 Street.
- 9) Existing boulevard trees along the west side of 106 Street, north of 99 Avenue should be retained and enhanced as part of any redevelopment of the former Safeway site.

9.5.3 Parking

- 1) Surface parking areas in the front yard of developments between the sidewalk and a building will be discouraged. On-site parking should be provided at the rear of buildings or underground.
- 2) Consider proposals for the reduction of required on-site parking for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct, in accordance with the parameters outlined in Section 2.33 of the Land Use Bylaw.
- 3) Consider the provision of accessible and secure bicycle parking facilities for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct.
- 4) As part of the redevelopment of a site, require any existing surface parking areas to be improved to comply with the standards of the Land Use Bylaw, in terms of hardsurfacing and landscaping, if these areas are not currently adhering to those standards.



9.5.4 Open Space

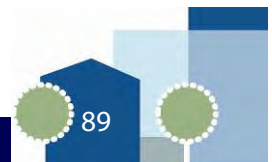
- 1) Explore opportunities for acquiring land or partnering with existing landowners for the development of small pocket parks fronting onto 104 Street, south of 99 Avenue, and 108 Street, in order to provide open space opportunities in the area.
- 2) Ensure redevelopment of the two mall sites provides a minimum of 15% of their area for on-site open space opportunities that are publicly accessible and integrated into the pedestrian connection network through the sites.
- 3) Consider the installation of an entrance feature on 99 Avenue where it intersects with 108 Street to bring attention to the fact that visitors are entering the Downtown.

9.5.5 Social/Cultural

- 1) Encourage public art installations and/or building murals in this Precinct to add vitality and interest to the public realm, particularly on the two mall sites.
- 2) Establish a Public Art Program that would require 1% of the constructions costs of new developments in this Precinct to be provided towards the purchase and installation of public art on a site.

9.5.6 Sustainability

- 1) Encourage new public buildings and facilities to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 2) Encourage new private developments to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 3) Explore opportunities to connect structures in the area to an alternative energy supply system, such as a district heating/cooling or geothermal system.
- 4) Conduct energy-efficiency audits on any City-owned buildings in the area and address identified deficiencies.
- 5) Consider capital projects in the area that meet the criteria of the Federation of Canadian Municipalities' Green Municipal Fund.
- 6) Encourage owners of commercial buildings in the area to apply for assistance in making energy-efficiency improvements to their buildings through the federal ecoENERGY Retrofit Program.





9.5.7 Economic Development

- 1) Explore the development of a Façade Improvement Program that provides municipal grants to business/property owners that undertake rehabilitation or improvements to the exterior façades of their buildings in this Precinct.
- 2) Encourage types of businesses not presently situated in the Downtown to locate in this Precinct.
- 3) Consider a reduction in the property tax assessment for businesses that are located in this Precinct.
- 4) Consider a cash-in-lieu payment for developments that propose a reduction in on-site parking in this Precinct.

9.6 DESIGN GUIDELINES

9.6.1 Background

The Mall Redevelopment Precinct is to be a mixed-use development area with an emphasis on higher density/higher scale residential and commercial development opportunities.

9.6.2 Buildings and Sites

Buildings with designated office/retail units at the base will be located along 99 Avenue facing the street with continuous retail frontages that will not be interrupted by vehicular access lane, surface parking lots or blank walls.

9.6.3 High-rise buildings (15 storeys or more)

- 1) High rise buildings will have three distinct vertical zones: base, middle, and top. The distinction is to be articulated through setbacks, differentiation of building materials, and/or architectural façade treatment.



A high-rise building is to have 3 distinct vertical zones and should be architecturally-treated to create an attractive building that is fitting within the context of Downtown Fort Saskatchewan.

- 2) The base (podium) of buildings will be a minimum of 2 storeys (8.5 m) and integrated with townhouses, mid-rise apartments, or commercial/retail units.



- 3) The middle zone of buildings will be setback a minimum of 3.0 m from the face of the podium portion along the street with a floor plate restriction of no more than 20.0 m x 30.0 m (600 m²).
- 4) The top zone of buildings, including the roof and penthouse, will be finished and architecturally treated to create an interesting roof line.
- 5) High rise buildings are to be located where they will not negatively impact (by shadowing and accelerated winds) the surrounding residential neighbourhood.

Discussion:

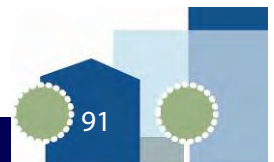
- *A slender point tower form is preferred as it has the least impact of shadowing of adjacent streets, open spaces, and neighbouring buildings.*
- 6) The base of high-rise buildings is to address the street that they front, and should locate primary building entrances at grade and on the public street frontage.

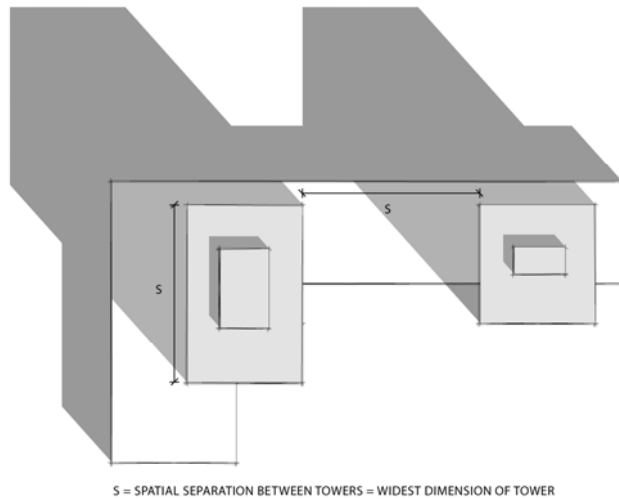
Discussion:

- *Articulate the base of the high-rise building at the primary entrance to allow the middle zone of the high-rise to meet the street in order to achieve a clear and visible address.*
- 7) A transitional method is to be presented between the high-rise and surrounding lower scaled neighbouring buildings, and open space.

Discussion:

- *The base of a high-rise is to be at the appropriate scale relative to adjacent buildings.*
 - *The base of a high-rise located near low density residential neighbourhoods will be limited to a maximum height of 2 storeys.*
 - *Articulation of the base should integrate with neighbouring buildings and the street, including appropriate ground floor uses for the area.*
 - *The scale and placement of high-rise buildings on a site should reduce its visible impact from streets, open spaces, and neighbouring properties, including the setback of upper portions of high-rise buildings from the base to allow for the base to serve as the primary definition of the street.*
 - *Use 45° angular planes to limit the height of high-rise buildings and to ensure that they step away from lower scaled buildings in the surrounding area.*
- 8) Where two towers are proposed, the minimum spacing between the shafts of two high-rises will equal the widest tower width measured perpendicularly to building face, but will be no less than 25.0 m.





Discussion:

- *Appropriate space between towers allows for appropriate light and privacy for new and existing buildings, as well as allowing appropriate sunlight, wind, and sky-views to the adjacent streets, parks, open spaces, and properties.*
- *The taller the building, the greater the facing distance between the buildings should be in order to achieve appropriate light and privacy.*
- *The taller the building, the greater the distance to the property line and neighbouring buildings on a site should be in order to achieve appropriate light and privacy.*

- 9) Where applicable, balconies are to be provided for all residential units.

Discussion:

- *Balconies may be permitted to project 1.0 m beyond the floor plate restriction.*
- 10) At grade, residential units are to have a 3.0 m front display garden, and the entrances to the units are to be raised a maximum of 1.0 m above the adjacent street. Entrances to residential units above are to be distinguishable from commercial entrances.
- 11) Building roofs will be designed to allow for their utilization as an outdoor communal space and to provide opportunities for rooftop gardens.
- 12) Vehicle entrance/exit access points to parking structures are not to be located along 99 Avenue.
- 13) Parking for residents, employees, and visitors is to be underground, enclosed, and/or fully screened.

9.6.4 Street and Sidewalk Environment

- 1) A continuous public pedestrian zone, a minimum of 3.4 m in width, measured from the front of the adjacent curb to the front face of the building on the ground floor, is to be provided for sites in this Precinct.

Discussion:

- *The 3.4 m pedestrian zone is comprised of a 0.9 m wide curbside zone for the placement of sidewalk furnishings, and a 2.5 m wide sidewalk.*



- *Where applicable, a building setback of a minimum of 1.4 m, measured from the front or side (for corner parcels) property line, may be required to achieve the continuous 3.4 m public pedestrian zone.*
- *The entire public pedestrian zone is to be hard-surfaced with a consistent surface pavement treatment from the public sidewalk to the building face.*

- 2) Streetscape elements such as banner flags, lighting, and hanging planters are to be coordinated along 99 Avenue and 100 Avenue. The streetscape elements will help distinguish the two “Main Streets” apart from one another and to create an interesting pedestrian environment.



Front entrances to at-grade residential units are to be raised a maximum of 1.0 m and are appropriately landscaped to provide privacy.

- 3) The street grid alignment is encouraged to be re-established within the Mall Redevelopment Precinct through the use of enhanced pedestrian corridor linkages.

Discussion:

- *The street grid alignment will create consistency throughout the Downtown providing an ease in navigating and pedestrian access from one part of the Downtown to another.*
- 4) Open public spaces within this Precinct are encouraged to provide public amenities to residences within the higher density development, and to establish an east-west connection of open spaces through the sites.



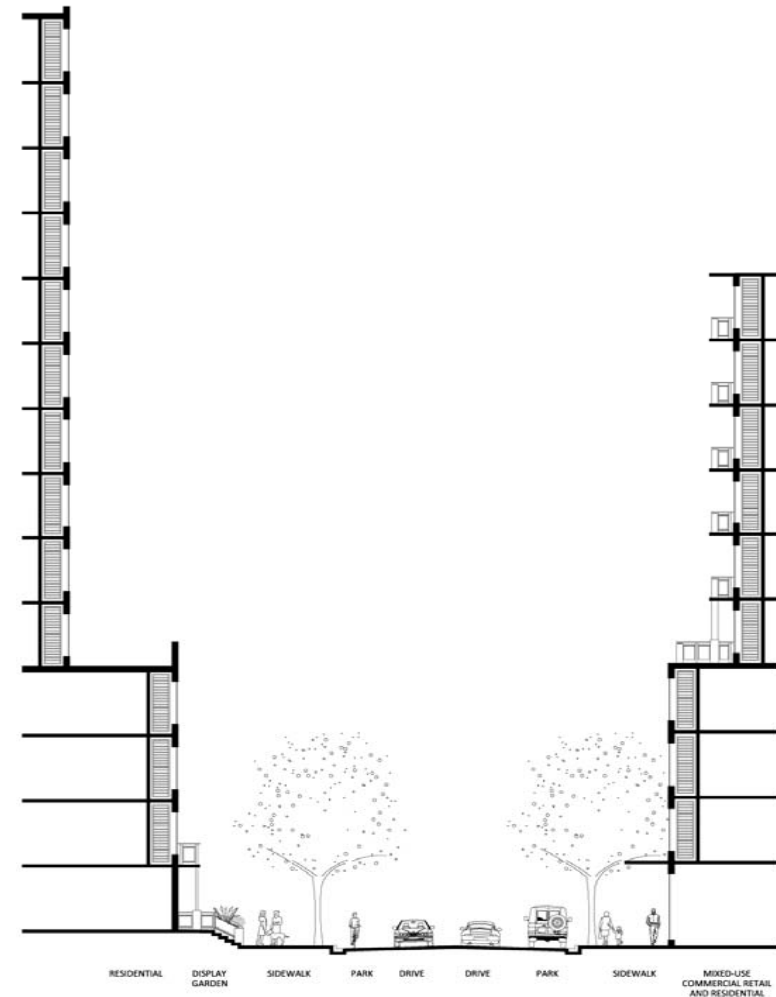
Provide open spaces within the Precinct to create a “green link” between existing open spaces.



- 5) Open public space types that are appropriate for this Precinct include esplanades, plazas, urban gardens, and squares.



The Mall Redevelopment Precinct should be developed with the intention of realigning the existing street grid through the use of enhanced pedestrian connections, and creating a diverse housing/commercial retail mix.



A street section illustrating the potential street and sidewalk environment within the Mall Redevelopment Precinct.



Street walls within this Precinct are to be at an appropriate height that is not overwhelming to the surrounding neighbourhood. Entrances to residential and/or commercial retail units should be clearly identified and differentiated.



10.0 NORTHEAST TRANSITION PRECINCT

10.1 CONTEXT

The Northeast Transition Precinct is located in the northeast corner of the Plan area. The area extends from 103 Street east to 106 Street, and from the lane south of 101 Avenue, it extends south to the lane south of 100 Avenue. Parcels in this Precinct on the south side of 100 Avenue are only located east of 104 Street. Surrounding development includes existing low density residential uses to the north and east, the Mall Redevelopment Precinct to the south, the Core Commercial Precinct to the southwest, and the Civic Precinct to the west (see Figure 7 - Precincts).

10.2 CHARACTER

The Precinct is located in an older area of Fort Saskatchewan. The area north of the lane that is north of 100 Avenue is developed with well-established low density residential uses. One or two vacant lots are present; however, the majority of the northern portion of the Precinct is a low density residential area. The majority of the dwellings in this area are in good condition, with well cared-for lots.

Parcels on the north and south side of 100 Avenue in the Precinct are developed with commercial and institutional uses. On the north side of 100 Avenue, commercial uses include office, retail, eating and drinking establishments, service, and the Brant Hotel. A Provincial courthouse is located at the east of the area, at the northeast corner of 100 Avenue and 105 Street. On the south side of 100 Avenue, a mix of uses is present. Commercial uses include office and retail; there are two parcels developed with low density residential uses, and a religious assembly is located in the middle of the block at 105 Street (see Figure 5 - Existing Uses).



The Northeast Transition Precinct accommodates a number of existing low density residential areas that are in close proximity to the commercial uses along 100 Avenue.



10.3 ISSUES AND OPPORTUNITIES

While this area has been a stable mix of residential and commercial over time, redevelopment opportunities in the Core Commercial and Mall Redevelopment Precincts may result in pressures for new development in this area. Issues and opportunities can be identified to assist in maintaining this area's character.

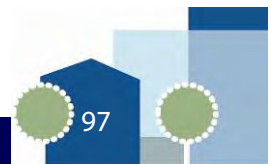
Established residential uses - the area north of the lane that is north of 100 Avenue is a well-established, low density residential area. The community has expressed a desire to retain the residential character of the area, and not accommodate expansion of the commercial area along 100 Avenue any further north of the lane.

Sustainable development - in conjunction with the broader goals of the Fort Saskatchewan Sustainability Plan, opportunities exist for new development in the area to achieve sustainable building and development principles.

Underutilization of land - current zoning for the commercial uses along 100 Avenue in the area provides the opportunity for commercial development up to approximately three storeys in height (or approximately eight storeys under certain circumstances), yet few buildings in the area take advantage of this opportunity. An increase in the intensification of development along 100 Avenue will help it to retain and increase its vitality as a major commercial street in the Downtown.

Transition between commercial and residential uses - given the allowances in height for the uses along 100 Avenue, and the fact that the area is in close proximity to a major commercial street, some limited residential intensification in the area is warranted. However, this intensification must provide an appropriate transition between the commercial uses on 100 Avenue, and the low density residential area further to the north and northeast. Residential redevelopment opportunities in the area will therefore be limited in height and density to ensure compatibility with surrounding single detached residential uses.

Maintenance and appearance of existing properties - a key element in creating and maintaining a thriving commercial core is ensuring that properties and buildings in the area provide an attractive and inviting environment for patrons and visitors alike. While many business storefronts along 100 Avenue are well-maintained, there are a number of buildings or storefronts that are falling into disrepair. Poor property and building maintenance shows a lack of pride of ownership, and over time, will lead to a decline in interest for new investment in the area, and in customers taking their business elsewhere.





Inconsistent streetscape elements - it has been noted that there has not been a comprehensive vision identified for public infrastructure in the Downtown area. As a result, there is a variety of styles of streetlights, planters, and other street furniture that do not contribute to a unified appearance for the area. There is also a lack of trees and other vegetation that can help make the area more attractive and inviting. To assist in making the Downtown a desirable area to invest in, a consistent and comprehensive approach to public realm improvements that will uniquely identify the area is needed.

Opportunities for mixed-use development - creating an environment that fosters a mix of commercial and residential development can assist greatly in ensuring an area is vibrant both day and night. Current zoning along 100 Avenue provides for residential uses to be developed over the ground floor of a building. This opportunity should be encouraged, where possible, for new developments.

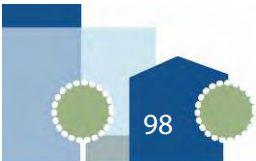
10.4 GENERAL INTENT OF THE PRECINCT

The general intent of the Northeast Transition Precinct is to retain the residential character of the area north of 100 Avenue. While some residential redevelopment opportunities are appropriate in this area, the intent is to limit them to semi-detached, duplex, or townhouse developments. Commercial uses along 100 Avenue will be retained, and redevelopment opportunities will be encouraged, including commercial buildings, and commercial/residential mixed-use buildings, where residential uses may be developed over ground floor commercial space (see Figure 10 - Development Concept).

10.5 POLICIES

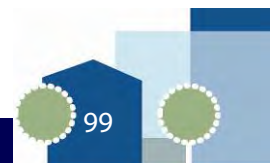
10.5.1 Land Use

- 1) The primary focus for the Precinct is to focus commercial development on 100 Avenue, with residential development occurring north of the lane that is north of 100 Avenue.
- 2) Infill redevelopment for residential uses north of the lane that is north of 100 Avenue will be limited to semi-detached dwellings, duplexes, and townhouses (up to four dwellings), to a maximum height of 2 ½ storeys (10.0 m). No commercial development will be permitted in this area. Infill residential uses must be sensitive to the existing character of the surrounding low density area.





- 3) Allow basement (or “granny” or “in-law”) suites as permitted uses in existing single detached dwellings as an additional means of increasing the density in the area.
- 4) Along 100 Avenue, encourage a variety of commercial and retail uses that are oriented to the street to make the area more attractive to pedestrians.
- 5) Along 100 Avenue, residential development that does not provide commercial uses on the ground floor will not be permitted.
- 6) Along 100 Avenue, mixed-use development is permitted, provided the ground floor of the building accommodates commercial development.
- 7) Along 100 Avenue, commercial uses shall have a maximum height of 4 storeys (14.0 m).
- 8) Along 100 Avenue, commercial development on the ground floor of buildings will have entrances that address the street and the pedestrian through the use of glazing, weather protection, signage, and lighting.
- 9) Support the provision of affordable housing units in residential developments in this Precinct in conjunction with applicable provincial and federal programs.
- 10) Uses along 100 Avenue should cater to both daytime and nighttime activities, in all seasons.
- 11) Along 100 Avenue, buildings should be built to the front property line, or to a building setback line as established in Section 10.6. A minimal setback may be permitted in order to accommodate active space associated with the related ground floor commercial use, such as patios or plazas.
- 12) Along 100 Avenue, the façades of buildings on corner sites must address both the abutting street and avenue.
- 13) Developments should adhere to Crime Prevention Through Environmental Design (CPTED) principles in order to make the area inviting for residents, employees, and visitors.
- 14) Along 100 Avenue, abrupt and excessive differences in building scale and massing should be minimized.
- 15) New vehicle-oriented uses, such as drive-throughs, will be discouraged.





10.5.2 Circulation

- 1) 100 Avenue between 101 Street and 104 Street will undergo an overall rehabilitation program in 2010. As part of this project, streetscaping and boulevard treatment improvements will be incorporated that assist in achieving aspects of the design guidelines for this area. The City will work with property owners on 100 Avenue to allow opportunities for input on the design and comments on specific proposed improvements. The City should also consider extending the surface streetscaping improvements on 100 Avenue from 104 Street to 106 Street to be consistent with the Plan boundaries.
- 2) 100 Avenue will continue to serve as a major access point into the Downtown, and will continue to function at a collector roadway standard.
- 3) Enhance pedestrian connections from the low-density residential area to the north of 101 Avenue to the Northeast Transition Precinct along 103 Street, 104 Street, and 105 Street to encourage better walkable connections into the Downtown.
- 2) Existing on-street parallel parking will remain to ensure an adequate supply of vehicular parking for those working in and visiting the area.
- 3) Consider resident-only parking passes for parking areas in front of residential areas to the north of the lane that is north of 100 Avenue, to ensure visitors to the commercial core of the Downtown are not parking in adjacent residential areas.
- 4) Along 100 Avenue, consider proposals for the reduction of required on-site parking for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct, in accordance with the parameters outlined in Section 2.33 of the Land Use Bylaw.
- 5) Consider the provision of accessible and secure bicycle parking facilities for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct.
- 6) As part of the redevelopment of a site, require any existing surface parking areas to be improved to comply with the standards of the Land Use Bylaw, in terms of hardsurfacing and landscaping, if these areas are not currently adhering to those standards.

10.5.3 Parking

- 1) Along 100 Avenue, surface parking areas in the front yard of developments between the sidewalk and a building will be discouraged. On-site parking should be provided at the rear of buildings or underground.





10.5.4 Open Space

- 1) Explore opportunities for acquiring land or partnering with existing landowners for the development of small pocket parks fronting onto 100 Avenue, in order to provide open space opportunities in the area.
- 2) Consider the installation of an entrance feature on 100 Avenue where it intersects with 106 Street to bring attention to the fact that visitors are entering the Downtown.

10.5.5 Social/Cultural

- 1) Existing historic resources should be protected and enhanced, through adaptive re-use.
- 2) Along 100 Avenue, encourage public art installations and/or building murals to add vitality and interest to the public realm.
- 3) Establish a Public Art Program that would require 1% of the constructions costs of new developments in this Precinct to be provided toward the purchase and installation of public art on a site.

10.5.6 Sustainability

- 1) Encourage new public buildings and facilities to pursue Leadership in Energy and Environmental Design (LEED®) certification.

- 2) Encourage new private developments to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 3) Explore opportunities to connect structures in the area, particularly along 100 Avenue, to an alternative energy supply system, such as a district heating/cooling or geothermal system.
- 4) Conduct energy-efficiency audits on any City-owned buildings in the area and address identified deficiencies.
- 5) Consider capital projects in the area that meet the criteria of the Federation of Canadian Municipalities' Green Municipal Fund.
- 6) Encourage owners of commercial buildings in the area to apply for assistance in making energy-efficiency improvements to their buildings through the federal ecoENERGY Retrofit Program.

10.5.7 Economic Development

- 1) Explore the development of a Façade Improvement Program that provides municipal grants to business/property owners on 100 Avenue that undertake rehabilitation or improvements to the exterior façades of their buildings in this Precinct.
- 2) Encourage types of businesses not presently situated in the Downtown to locate in this Precinct.



- 3) Consider a reduction in the property tax assessment for businesses that are located in this Precinct.
- 4) Consider a cash-in-lieu payment for developments that propose a reduction in on-site parking in this Precinct.

10.6 DESIGN GUIDELINES

10.6.1 Background

The Northeast Transition Precinct is to be primarily residential in nature with the opportunity of supportive establishment uses such as daycares. A variety of low density residential building types ranging from semi-detached dwellings to townhouses is encouraged.

10.6.2 Buildings and Sites

- 1) Higher density buildings and supportive uses will be located along 100 Avenue, respecting existing residential uses along the perimeter of the boundary of this Precinct.
- 2) Ground floor residential units are to have individual entrances to the street to emphasize the residential nature of the area.
- 3) Residential units located on the ground floor are to locate the main floor at a maximum of 1.0 m above the adjacent street level to promote privacy for the units, yet still maintain 'eyes on the street' from the unit. These entrances are to be spaced frequently and be combined

with steps, terraces, or stoops.



Provide opportunities for supporting uses such as daycares to be integrated in developments in the Northeast Transition Precinct.

- 4) Where applicable, ground floor residential lobby entrances are to be level with the grade of the adjacent sidewalk, or accessed by a ramp in order to be universally accessible. Shared residential entrances are to be distinguishable from the private residential entrances of ground floor units.
- 5) Residential developments are to be set back a minimum of 3.0 m from the front property line. This setback is to accommodate appropriate landscaping and other amenity area space for the respective dwelling unit(s).
- 6) Where applicable, the back walls of buildings that face onto residential developments are to be architecturally treated to avoid creating large blank walls.
- 7) All parking areas are to be located at the rear of buildings to ensure the sidewalks/pathways and building façades effectively define the street edge.

Discussion:

- *The parking areas are to be underground, enclosed, or fully screened from view.*

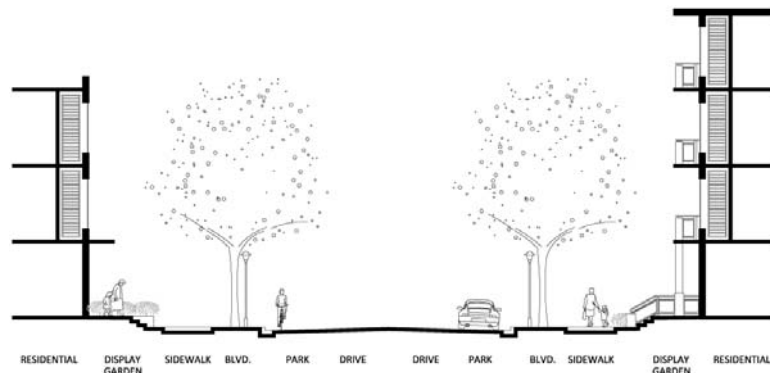


10.6.3 Street and Sidewalk Environment

- 1) For 100 Avenue, a continuous public pedestrian zone, a minimum of 4.0 m in width, measured from the front of the adjacent curb to the front face of the building on the ground floor, is to be provided for sites in this Precinct.

Discussion:

- The 4.0 m pedestrian zone is comprised of a 1.5 m wide grassed and tree-lined boulevard, a 2.0 m wide sidewalk, and a 0.5 m wide landscaped transition zone.
- Where applicable, a building setback of a minimum of 2.2 m, measured from the front or side (for corner parcels) property line, may be required to achieve the continuous 4.0 m public pedestrian zone.



A street section illustrating the potential street and sidewalk environment within the Northeast Transition Precinct.



Residential development within this Precinct should address the adjacent street and have entrances off of the sidewalk. Surface parking is to be provided at the rear of buildings to preserve the continuous street frontage.



11.0 CIVIC PRECINCT

11.1 CONTEXT

The Civic Precinct is located in the north central portion of the Plan area. The area is bounded by 101 Avenue to the north, 103 Street to the east, 100 Avenue to the south, and 101 Street to the west. Surrounding development includes existing low density residential uses to the north (including a senior's lodge) and northeast, the Northeast Transition Precinct to the east, the Core Commercial Precinct to the south, and the Old Fort/Open Space Precinct to the west (see Figure 7 - Precincts).

11.2 CHARACTER

This Precinct serves as a primary focus point for public services and activities. City Hall (located on the east side of 102 Street), the Fort Saskatchewan Public Library (at the southeast corner of the intersection of 102 Street and 101 Avenue), a Canada Post outlet (located on the west side of 103 Street), and Pioneer House (located at the northeast corner of the intersection of 101 Street and 100 Avenue), a drop-in social and learning centre for senior citizens, are all located in this Precinct.

Medium density residential uses are developed along the east side of 101 Street, to a height of three and four storeys. Along the north side of 100 Avenue between 102 Street and 103 Street is a range of commercial uses providing retail, office, service, and eating and drinking establishment uses, located in older storefronts. A large, municipally-owned parking lot is located on the west side of 102 Street across from City Hall (see Figure 5 - Existing Uses).

This Precinct is also the location of the proposed City Hall Plaza project. This project is an initiative that centres on the existing City Hall, a new, three storey municipal building, the Fort Saskatchewan Public Library, and the public spaces around and in between these facilities. The intent is to have City Hall Plaza reflect the character and aspirations of the City of Fort Saskatchewan, provide a high quality working environment, an inviting public space, and be an anchor for Downtown redevelopment.



The Civic Precinct is home to a number of public service facilities including City Hall, the Public Library and the main Canada Post outlet.



11.3 ISSUES AND OPPORTUNITIES

City Hall Plaza project - the development of a new municipal building, and the associated improvements in the Plaza project area, will set the tone for the expectations of redevelopment in the overall Downtown and will create a focused activity area. Consolidation of City administrative staff into the area will also provide additional people into the Downtown area on a daily basis, benefitting businesses and services.

Central area for public services - the City Hall Plaza project and the existing public service uses such as City Hall, the Library, and the Canada Post outlet all provide convenient services in a central location to residents and those looking to do business in or with the City.

Inconsistent streetscape elements - it has been noted that there has not been a comprehensive vision identified for public infrastructure in the Downtown area. As a result, there is a variety of styles of streetlights, planters, and other street furniture that do not contribute to a unified appearance for the area. There is also a lack of trees and other vegetation that can help make the area more attractive and inviting. To assist in making the Downtown a desirable area to invest in, a consistent and comprehensive approach to public realm improvements that will uniquely identify the area is needed.

Maintain residential development - medium density residential developments in the area along 101 Street and 101 Avenue contribute towards the Downtown population base, and should be retained.

Sustainable development - in conjunction with the broader goals of the Fort Saskatchewan Sustainability Plan, opportunities exist for new development in the area to achieve sustainable building and development principles.

11.4 GENERAL INTENT OF THE PRECINCT

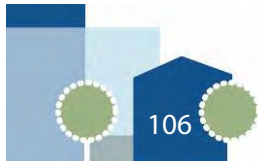
The general intent of the Civic Precinct is to continue to accommodate municipal services and buildings that serve not just residents in the Downtown, but the entire community. In conjunction with the City Hall Plaza project, the area will provide an inviting public space and serve as a focus for activity in the Downtown. Residential uses along 101 Street and commercial uses along 100 Avenue will also be accommodated to create a vibrant mix of uses in the area (see Figure 10 - Development Concept).



11.5 POLICIES

11.5.1 Land Use

- 1) Encourage the development of a City Hall Plaza project as a means to create a central focus for public services and a public open space opportunity.
- 2) Retain and enhance the Pioneer House facility as a key gathering place in the Downtown for local seniors and other residents or community groups.
- 3) Maintain, enhance, and consider expansion of the Fort Saskatchewan Public Library facility in its current location to continue its presence Downtown.
- 4) Consider a redevelopment opportunity for the Canada Post office site, provided a suitable location for the facility can be found elsewhere.
- 5) The maximum building height in this Precinct shall be 4 storeys (14.0 m).
- 6) Along 100 Avenue, commercial development on the ground floor of buildings will have entrances that address the street and the pedestrian through the use of glazing, weather protection, signage, and lighting.
- 7) Along 100 Avenue, mixed-use development is permitted, provided the ground floor of the building accommodates commercial development.
- 8) Support the provision of affordable housing units in residential developments in this Precinct in conjunction with applicable provincial and federal programs.
- 9) Buildings should be built to the front property line, or to a building setback line as established in Section 11.6. A minimal setback may be permitted in order to accommodate active space associated with the related ground floor commercial use, such as patios or plazas.
- 10) The façades of buildings on corner sites must address both the abutting street and avenue.
- 11) Developments should adhere to Crime Prevention Through Environmental Design (CPTED) principles in order to make the area inviting for residents, employees, and visitors.
- 12) New vehicle-oriented uses, such as drive-throughs, will be discouraged.



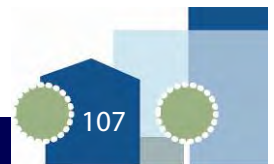


11.5.2 Circulation

- 1) 100 Avenue between 101 Street and 104 Street will undergo a rehabilitation program in 2010. As part of this project streetscaping and boulevard treatment improvements will be incorporated to achieve aspects of the design guidelines for this area, including on the City Hall Plaza site. The City will work with property owners on 100 Avenue to allow opportunities for input on the design and comments on specific proposed improvements.
- 2) Streetscape improvements that are consistent with the concept for 100 Avenue should be considered for 102 Street and 103 Street.
- 3) Consider the permanent closure of 102 Street between 100 Avenue and 101 Avenue to vehicle traffic as part of the City Hall Plaza project, in order to create a pedestrian corridor through this significant public amenity area.
- 4) 100 Avenue and 101 Street will continue to serve as major access points into the Downtown. 101 Street will continue to function at an arterial roadway standard, while 100 Avenue will function at a collector roadway standard.
- 5) Enhance pedestrian and bicycle connections from the Civic Precinct to the Old Fort/Open Space Precinct to the west, and the North Saskatchewan River valley to the north to encourage users of these open space areas to travel into the Downtown.

11.5.3 Parking

- 1) Surface parking areas in the front yard of developments between the sidewalk and a building will be discouraged. On-site parking should be provided at the rear of buildings or underground.
- 2) As part of the City Hall Plaza development, consider the provision of a City-owned and operated parking facility to serve patrons utilizing the public services in the area.
- 3) Existing on-street parallel parking on 100 Avenue and 103 Street will remain to ensure an adequate supply of vehicular parking for those working in and visiting the area.
- 4) Consider proposals for the reduction of required on-site parking for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct, in accordance with the parameters outlined in Section 2.33 of the Land Use Bylaw.
- 5) Consider the provision of accessible and secure bicycle parking facilities for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct.





- 6) As part of the redevelopment of a site, require any existing surface parking areas to be improved to comply with the standards of the Land Use Bylaw, in terms of hardsurfacing and landscaping, if these areas are not currently adhering to those standards.

11.5.4 Open Space

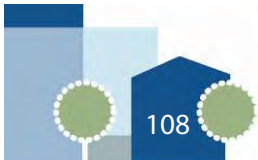
- 1) Provide an open space component with the City Hall Plaza project and potential pedestrian-oriented corridor along 102 Street to serve local employers and residents in the area.
- 2) Consider the installation of an entrance feature on 101 Street where it intersects with 101 Avenue to bring attention to the fact that visitors are entering the Downtown.

11.5.5 Social/Cultural

- 1) Existing historic resources should be protected and enhanced, through adaptive re-use.
- 2) Encourage public art installations and/or building murals in this Precinct to add vitality and interest to the public realm, particularly as part of the City Hall Plaza project.
- 3) Establish a Public Art Program that would require 1% of the constructions costs of new developments in this Precinct to be provided towards the purchase and installation of public art on a site.

11.5.6 Sustainability

- 1) Encourage new public buildings and facilities to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 2) Encourage new private developments to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 3) Explore opportunities to connect structures in the area, particularly along 100 Avenue, to an alternative energy supply system, such as a district heating/cooling or geothermal system.
- 4) Conduct energy-efficiency audits on any City-owned buildings in the area and address identified deficiencies.
- 5) Consider capital projects in the area that meet the criteria of the Federation of Canadian Municipalities' Green Municipal Fund.
- 6) Encourage owners of commercial buildings in the area to apply for assistance in making energy-efficiency improvements to their buildings through the federal ecoENERGY Retrofit Program.





11.5.7 Economic Development

- 1) Explore the development of a Façade Improvement Program that provides municipal grants to business/property owners on 100 Avenue that undertake rehabilitation or improvements to the exterior façades of their buildings in this Precinct.
- 2) Consider a reduction in the property tax assessment for businesses that are located in this Precinct.
- 3) Consider a cash-in-lieu payment for developments that propose a reduction in on-site parking in this Precinct.

11.6 DESIGN GUIDELINES

11.6.1 Background

The Civic Precinct is to be primarily institutional in nature with the opportunity of office/retail uses at street level along 100 Avenue, and medium density residential uses along 101 Street. The public buildings within this Precinct will be institutional and/or civic in their architectural expression, exhibiting traits of an iconic architecture, one that sets them apart from all the buildings in the Downtown, but also fitting within the context.

11.6.2 Buildings and Site

Public buildings will achieve a minimum of a LEED® Silver certification level.

All surface parking areas are to be located at the rear of buildings to ensure the sidewalks/pathways and building façades effectively define the street edge.

Discussion:

- *The parking areas ideally should be underground or enclosed.*



The buildings within the Civic Precinct will exhibit traits of an iconic architecture, one that sets them apart from all buildings in the Downtown, while also fitting within the context of the area.



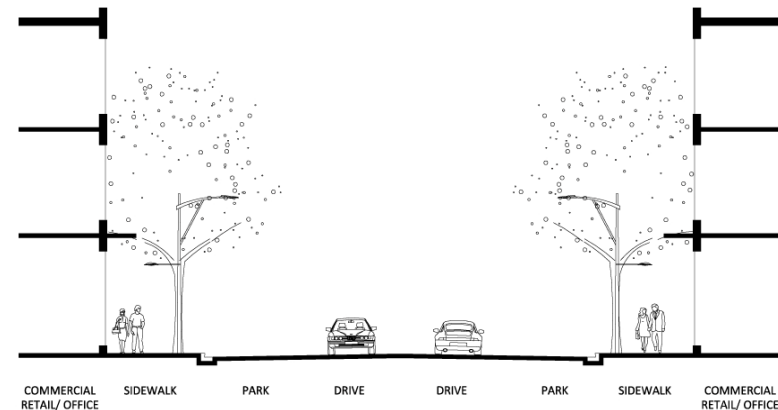
11.6.3 Street and Sidewalk Environment

- 1) A continuous public pedestrian zone, a minimum of 3.4 m in width, measured from the front of the adjacent curb to the front face of the building on the ground floor, is to be provided for sites in this Precinct.

Discussion:

- The 3.4 m pedestrian zone is comprised of a 0.9 m wide curbside zone, for the placement of sidewalk furnishings, and a 2.5 m wide sidewalk.
 - Where applicable, a building setback of a minimum of 1.1 m, measured from the front or side (for corner parcels) property line, may be required to achieve the continuous 3.4 m public pedestrian zone.
 - The entire public pedestrian zone is to be hard-surfaced with a consistent surface pavement treatment from the public sidewalk to the building face.
- 2) The streets within this Precinct will be lined with street trees planted in tree grates at intervals of 6.0 - 8.0 m.
 - 3) Streetscape elements such as banner flags, lighting, and hanging planters are to be coordinated along 100 Avenue. The streetscape elements will help distinguish this roadway as one of the two "Main Streets" in the Downtown, and to create an interesting pedestrian environment.

- 4) Open public spaces, such as plazas, urban gardens, and/or squares, are to be provided within this Precinct, with the potential of the space to be utilized for seasonal markets or as an outdoor gathering place.



A street section illustrating the potential street and sidewalk environment within the Civic Precinct.



12.0 OLD FORT / OPEN SPACE PRECINCT

12.1 CONTEXT

The Old Fort/Open Space Precinct is located in the northwest portion of the Plan area. The area is bounded by the North Saskatchewan River valley to the north, 101 Street to the east, 99 Avenue to the south, and approximately 100 Street to the west. Surrounding development includes the North Saskatchewan River valley and associated trail system to the north, the Civic and Core Commercial Precincts to the east, an open space/schools area and the 99th Commercial Precinct to the south, and an established medium density residential area to the west (see Figure 7 - Precincts).

12.2 CHARACTER

This Precinct's connection to Fort Saskatchewan goes back to the City's origins, with the original fort being located in this area in 1875 by the Northwest Mounted Police. While providing historical ties for the community, the area also serves as a significant open space and recreation area for citizens. The original CN Rail station to serve Fort Saskatchewan was built here in 1905, just north of the present day 99 Avenue, just west of 101 Street. The station remains in its original location, in addition to a 1957-era caboose. This area was also the site for the former provincial gaol, originally located here in 1915 (closed in 1988, buildings demolished in 1994). The former Warden's House, built in 1937, is still located in the area, on the north side of 100 Avenue, west of 101 Street.



The CN Rail station, built in 1905 to serve Fort Saskatchewan, is still located on its original site in this area.

The Fort Saskatchewan Museum and Historic Site is located in the northeastern portion of the Precinct, just west of 101 Street. It contains a number of historic buildings, including the Old Courthouse, built in 1909.

Other uses in the area include a single detached dwelling on the west side of 101 Street, just south of the Museum, and a religious assembly at the northeast corner of the intersection of 101 Street and 100 Avenue. The City Community Services Department utilizes an older institutional building located near the original fort site, north of 100 Avenue, just east of the existing medium density residential uses at the western edge of the area.



The former Warden's House, originally built in 1937, was part of the former provincial gaol located in this area.



The Legacy Park Band Shell is also located in the area, on the south side of 100 Avenue. The City's Sheep Grazing Program is also based out of the area, with the sheep often grazing in Legacy Park, or near the Museum and Warden's House. The balance of the area is used for open space and other passive recreation opportunities, and is a popular location for community events and festivals year round, including Canada Day celebrations and a weekly Farmer's Market (during the summer months) (see Figure 5 - Existing Uses).

12.3 ISSUES AND OPPORTUNITIES

Connections to community's history - as the site of the original fort, former provincial gaol, and the original CN Rail station, the area has many ties to the very origins of Fort Saskatchewan. Many of the structures associated with this history have fortunately been retained, and form an excellent foundation on which to expand on interpreting the community's past.

Central focus for open space activities - the area provides a well-established and successful open space feature that serves not only the Downtown, but the entire community. Opportunities exist to enhance this key asset to the Downtown for outdoor activity and cultural events.

Proximity to Downtown core - the Precinct's location adjacent to the core commercial and residential areas of the Downtown provides an immediate amenity for residents and visitors to the area. However, few users of the park spaces tend to migrate into the Downtown during festivals or other activities. Better integration of the open space and cultural facilities with the Downtown area would help facilitate increased activity in the core area, particularly when specific events are taking place.

Linkages to North Saskatchewan River valley - not only does the Precinct's open space provide an excellent amenity for the Downtown, it is also located on the banks of the River valley. This location provides many opportunities for trail linkages to and from the Downtown, but also provides excellent viewpoint sites.

12.4 GENERAL INTENT OF THE PRECINCT

The general intent of the Old Fort/Open Space Precinct is to maintain and enhance the existing open space and cultural facilities found in the area. The importance of these assets to the success of the Downtown should not be underestimated. Opportunities for new activities that utilize the area on a year-round basis will be supported, and improved integration and interaction between this area and the Downtown core will be encouraged (see Figure 10 - Development Concept).



12.5 POLICIES

12.5.1 Land Use

- 1) The primary focus for the Precinct is to accommodate existing and future open space, historic, and other cultural activities that serve both Downtown residents and visitors, but also the entire City.
- 2) Open space or building developments should adhere to Crime Prevention Through Environmental Design (CPTED) principles in order to make the area inviting for residents and visitors.
- 3) New residential uses will not be permitted in the area. Small commercial/retail uses, to a maximum size of 75 m² may be permitted along 100 Avenue, west of 101 Street.

12.5.2 Circulation

- 1) 100 Avenue between 101 Street and 104 Street will undergo an overall rehabilitation program in 2010. Consideration should be made to extending some or all of the streetscape improvements to 100 Avenue west of 101 Street for a small distance to promote better integration between this area and the adjacent Precincts.

- 2) 100 Avenue will continue to serve as a primary access into the area, and will continue to function at a collector road standard.
- 3) Enhance pedestrian connections from the Old Fort/Open Space Precinct to the Civic, Core Commercial, and 99th Commercial Precincts to ensure ease of movement between these areas.
- 4) Enhance pedestrian connections from the Precinct to the trail network in the North Saskatchewan River valley to encourage users of these open space areas to travel into the Downtown.
- 5) Enhance pedestrian connections from the Museum site into the balance of the Old Fort/Open Space Precinct and across 101 Street, connecting to 101 Avenue, to provide better access to the Civic Precinct and the balance of the Downtown.

12.5.3 Parking

- 1) Consider the development of additional parking areas in this Precinct that are municipally-owned and operated to serve the public.

12.5.4 Open Space

- 1) Maintain and enhance the existing Jarvis, Legacy, and Peter Ream Parks in the area.



- 2) Explore opportunities for new park development near the site of the original Northwest Mounted Police fort on the north side of 100 Avenue.
- 3) Provide viewpoint features with seating areas, along the top of the bank of the North Saskatchewan River.
- 4) Encourage the continuation of the Legacy Park Snowbank Rink and support additional outdoor use of the open spaces in the area in the winter months.

12.5.5 Social/Cultural

- 1) The Museum and its historic buildings and other assets should be maintained, enhanced, and possibly expanded to celebrate and encourage an appreciation of Fort Saskatchewan's history.
- 2) Maintain the Sheep Grazing Program in the area as a popular local and visitor attraction.
- 3) Maintain and enhance the CN Rail station site to provide more information on the importance of the railway's impact on the development of the community.
- 4) Consider applying for a Provincial Historic District designation to be provided for the Museum and old fort sites to assist in recognizing and preserving the community's history.
- 5) Maintain the Legacy Park Band Shell for use in conjunction

with community festivals and other entertainment activities.

- 6) Support and look for opportunities to expand the Farmer's Market program at Legacy Park, but also consider the relocation of the Market (or an extension of the Market) to the Downtown core.
- 7) Maintain existing and provide additional historical interpretation panels throughout the area to provide information on and celebrate the rich history of the community.
- 8) Encourage public art installations and/or building murals to non-historic structures to add vitality and interest to the public realm.

12.5.6 Sustainability

- 1) Encourage any new public buildings and facilities to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 2) Explore opportunities to locate an alternative energy supply system, such as a district heating/cooling or geothermal system, in this area to assist in providing energy needs to the Downtown core.
- 3) Conduct energy-efficiency audits on any City-owned buildings in the area and address identified deficiencies.



- 4) Consider capital projects in the area that meet the criteria of the Federation of Canadian Municipalities' Green Municipal Fund.
- 5) Maintain the Sheep Grazing Program as a cost-effective and environmentally-friendly means of maintaining the park lands in the area.

12.5.7 Economic Development

- 1) Promote the Museum, Warden's House, and CN Rail Station sites as important historical assets and include them in tourist literature for visitors to the community.

12.6 DESIGN GUIDELINES

12.6.1 Background

The Old Fort/Open Space Precinct is to be preserved as a public open space that situates it as a destination for not only the Downtown, but the entire community.

Discussion:

- *Potential uses include historic interpretation, public park, and outdoor gathering places.*
- *60% of the open space in the Precinct is to be developed through sustainable practices, providing wetlands and native planting.*
- *The Precinct will continue to accommodate various uses and activities ranging from sitting, viewing of sheep grazing, walking,*

and ball playing, but also accommodate additional community activities.

The perimeter of the Precinct along 101 Street and 99 Avenue will be landscaped to provide a visual and auditory buffer.

General strategies for public open spaces:

- achieve a distinct public realm character that fits with the overall streetscape of Downtown Fort Saskatchewan;
- integrate stormwater management features into the design of the area;
- explore opportunities to improve the public realm and pedestrian environment along 99 Avenue and 101 Street;
- integrate public art in the public and semi-public spaces; and
- create visual interest and view opportunities in public spaces at key entrances, including 99 Avenue, 100 Avenue, and 101 Street.



13.0 99TH COMMERCIAL PRECINCT

13.1 CONTEXT

The 99th Commercial Precinct is located in the southwestern portion of the Plan area. The area is bounded by open space and residential development to the north and east, 99 Avenue to the south, and Sherridon Drive to the west. Surrounding development includes low scale, medium density residential development and the Old Fort/Open Space Precinct to the north and east, low density residential across 99 Avenue to the south, and low scale, medium density residential to the west (see Figure 7 - Precincts).

13.2 CHARACTER

This Precinct provides a relatively new and stable commercial area in the Downtown. Commercial development includes retail, office, service, entertainment, and eating and drinking establishments. Buildings are generally oriented to vehicular traffic, with large surface parking areas abutting 99 Avenue, and the buildings are set back quite far from the street. The majority of the retail uses are contained in large format stores, such as a Sobey's, a Shopper's Drug Mart, and an Alberta Treasury Branch.



The 99th Commercial Precinct features a number of well-established commercial and retail uses, with larger-format stores that cater to vehicular traffic.

A two storey office building has been recently developed in the area as well. While sidewalks are present along 99 Avenue and Sherridon Drive, the area is not generally conducive to pedestrians, and is primarily set up to accommodate vehicle traffic. Access to the area is taken from 100 Street or Sherridon Drive (see Figure 5 - Existing Uses).

13.3 ISSUES AND OPPORTUNITIES

Generally stable commercial area - buildings and commercial uses in the area are relatively new and stable. The area is well-used, and there are no apparent vacancies in the buildings on the site. As such, no significant redevelopment opportunities are present at this time.

Arterial roadway location - this Precinct's location along 99 Avenue, an arterial roadway, provides a good site for commercial uses that require larger-format buildings to serve a high volume of customer traffic. These types of uses tend to require large sites with ample surface parking, which would be difficult to provide in the Downtown core, outside of the Mall Redevelopment Precinct.



Vehicle-oriented nature of development - while the site's location adjacent to the 99 Avenue arterial roadway is beneficial in some aspects, it does result in a vehicle-oriented environment which does not accommodate pedestrian traffic particularly well. Better linkages from the site to adjacent sidewalks and to adjacent development to the north would assist in making the area more welcome for pedestrians.

Abutting low scale residential development - the existing low scale, medium density residential development and the open space area to the north place some limitations on the redevelopment opportunity for this site. Future uses and building scale will need to be sensitive to the adjacent uses.

Adjacent open space area - while the adjacent open space area to the north in the Old Fort/Open Space Precinct may pose some limitations on the redevelopment opportunity for the area, it also provides a good opportunity for businesses to cater to users of the park space. More formalized linkages between the two Precincts and sensitive building design would assist in encouraging more pedestrian movements.

13.4 GENERAL INTENT OF THE PRECINCT

The general intent of the 99th Commercial Precinct is to maintain and enhance the existing larger format commercial uses, and preserve it as an area suitable for more vehicle-oriented uses, given its location along the 99 Avenue arterial roadway. Better on-site pedestrian facilities are encouraged for the area, and improved pedestrian linkages between the Precinct and the uses to the north are considered to be beneficial (see Figure 10 - Development Concept).

13.5 POLICIES

13.5.1 Land Use

- 1) The primary focus for the Precinct is to accommodate commercial development, including retail, office, service, entertainment, and eating and drinking establishments.
- 2) Despite the vehicle-oriented nature of the area, encourage new commercial uses that are oriented to the street to make the area more attractive to pedestrians.
- 3) Mixed-use development with residential uses located above ground floor commercial space will be permitted in this Precinct.
- 4) The maximum building height in this Precinct shall be 4 storeys (14.0 m).





- 5) Commercial development on the ground floor of buildings will have entrances that address the street and the pedestrian through the use of glazing, weather protection, signage, and lighting.
- 6) The area should cater to both daytime and nighttime activities, in all seasons.
- 7) Developments should adhere to Crime Prevention Through Environmental Design (CPTED) principles in order to make the area inviting for residents, employees, and visitors.

13.5.2 Circulation

- 1) Streetscape improvements should be considered along both sides of 100 Street from 99 Avenue north to 100 Avenue to encourage better pedestrian access to the area.
- 2) Provide enhanced on-site pedestrian connections to encourage people to patronize commercial uses on foot.
- 3) 99 Avenue will continue to serve as one of the major access points into the Downtown, and will continue to function at an arterial roadway standard.
- 4) Provide new pedestrian connections from the residential and open space areas to the north of the Precinct to promote additional pedestrian traffic into the area.

13.5.3 Parking

- 1) Provide additional landscaping on-site and along 99 Avenue for surface parking areas to soften the appearance of these large areas.
- 2) Surface parking areas adjacent to streets and avenues will be considered in this area provided a suitable interface with the abutting street, including substantial screening of vehicles, is included.
- 3) Consider proposals for the reduction of required on-site parking for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct.
- 4) Consider the provision of accessible and secure bicycle parking facilities for new developments, redevelopments, or additions or enlargements to existing buildings in this Precinct.

13.5.4 Open Space

- 1) Provide on-site, outdoor seating areas for patrons of the businesses in the Precinct in conjunction with improved pedestrian connections and on-site landscaping.
- 2) Consider the installation of an entrance feature on 99 Avenue where it intersects with 100 Street to bring attention to the fact that visitors are entering the Downtown.



13.5.5 Social/Cultural

- 1) Encourage public art installations and/or building murals to non-historic structures to add vitality and interest to the public realm.
- 2) Establish a Public Art Program that would require 1% of the constructions costs of new developments in this Precinct to be provided towards the purchase and installation of public art on a site.

13.5.6 Sustainability

- 1) Encourage new public buildings and facilities to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 2) Encourage new private developments to pursue Leadership in Energy and Environmental Design (LEED®) certification.
- 3) Explore opportunities to connect structures in the area to an alternative energy supply system, such as a district heating/cooling or geothermal system.
- 4) Conduct energy-efficiency audits on any City-owned buildings in the area and address identified deficiencies.
- 5) Consider capital projects in the area that meet the criteria of the Federation of Canadian Municipalities' Green Municipal Fund.

- 6) Encourage owners of commercial buildings in the area to apply for assistance in making energy-efficiency improvements to their buildings through the federal ecoENERGY Retrofit Program.

13.5.7 Economic Development

- 1) Explore the development of a Façade Improvement Program that provides municipal grants to business/property owners that undertake rehabilitation or improvements to the exterior façades of their buildings in this Precinct.
- 2) Consider a reduction in the property tax assessment for businesses that are located in this Precinct.
- 3) Consider a cash-in-lieu payment for developments that propose a reduction in on-site parking in this Precinct.

13.6 DESIGN GUIDELINES

13.6.1 Background

The 99th Commercial Precinct is to be primarily commercial that will not compete with uses in the Core Commercial Precinct. Residential units above an office/retail base will be allowed.

The types of retail that will be accommodated in the 99th Commercial Precinct are those that require a large site (due to display and sales of large bulky products) and the use of a personal vehicle.



13.6.2 Buildings and Sites

- 1) Buildings are to be built to the landscape setback line of 12.0 m, measured from the curb along 99 Avenue, and are to be located and oriented to front and face the adjacent public thoroughfare.

Discussion:

- *The landscaped setback will provide a transition and buffer from the street as well as encourage pedestrian access to uses on sites within this Precinct.*
- 2) The width and depth of an individual building will not exceed 40.0 m and 20.0 m respectively.

Discussion:

- *The large building will be located behind or above smaller frontages, whereby the 'liner stores' will occupy a floor space of a maximum of 100 m².*
- *A clearly visible front entrance is to be provided for each individual store at grade adjacent to the street.*
- *Front entrances will be located within 6.0 - 12.0 m of another entrance.*
- *All entrances are to front and face the adjacent public thoroughfare to avoid creating an internalized "mall".*



A large tenancy is to be located behind or above smaller individual tenancies that address the pedestrian environment. The building is to be highly transparent to encourage pedestrian interaction and safety

- 3) Façades facing a public street or public area are to incorporate a minimum of 60% glazing at grade and within the first two storeys to encourage pedestrian interaction and safety.
- 4) Where applicable, the back walls of buildings that face onto residential developments or the Old Fort/Open Space Precinct are to be architecturally treated to avoid creating large blank walls.

Discussion:

- *In addition, a landscaped buffer zone with the use of earth berms and planting will be provided between the back walls of buildings and adjacent areas to the north.*



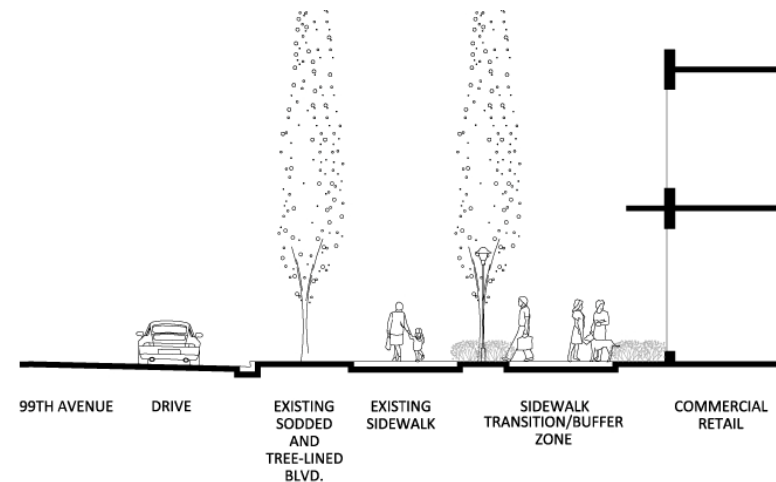
- 5) Large billboard signs that do not respond to the pedestrian environment will not be allowed.
- 6) All vehicular access to parking and on-site passenger facilities, loading, and service areas are to be from side streets or back lane, reducing interruptions on 99 Avenue.
- 7) Vehicular entrances are to be consolidated to serve multiple buildings in each block to minimize the number of interruptions in the street wall and to reduce the number of potential conflicts with pedestrians and cyclists.
- 8) Outdoor storage areas to be located at the rear of buildings, and screened from 99 Avenue, adjacent residential uses, and public open spaces through landscaping treatments.
- 9) Surface parking lots are to be landscaped at the street edge to minimize their visual impact and to improve the pedestrian environment.

Discussion:

- *Landscaped islands and edges, as well as porous paving materials will reduce the amount of impervious surfaces and increase the aesthetics of surface parking areas.*
- *One tree per 20 m² and one shrub per 10 m² in each parking island,*
- *Where applicable, surface parking areas will be set back a minimum of 1.5 m from an adjacent sidewalk to provide a landscaped screen.*



A landscaped edge and parking island will minimize the visual impact of surface parking and increase rainwater permeability.



A street section illustrating the potential street and sidewalk environment within the 99th Commercial Precinct.



13.6.3 Street and Sidewalk Environment

- 1) A continuous public pedestrian zone, a minimum of 12.0 m in width, measured from the front of the adjacent curb to the front face of the building on the ground floor, is to be provided along 99 Avenue.

Discussion:

- *The 12.0 m pedestrian zone is comprised of the existing 3.0 m sidewalk and a grassed and tree-lined boulevard. In addition, a minimum of a 1.5 m wide landscaped buffer zone and a second 3.0 m wide sidewalk is to be provided.*
- *The existing trees and sidewalk paving are to be protected and incorporated into the site design.*
- *The sidewalk is to be hard-surfaced with a consistent surface pavement treatment to that of the public sidewalk.*
- *The boulevard and buffer zone will be lined with street trees at intervals of 6.0 m.*
- *The buffer zone will be landscaped with plant materials other than grass.*



A landscaped buffer provides a safer pedestrian environment on the sidewalk along a busy street.

- 2) A minimum of a 2.5 m sidewalk will be provided along all building frontages in the area.
- 3) The streetscaping scheme along 99 Avenue will have emphasis on providing a sense of entry into the Downtown.



14.0 IMPLEMENTATION PROGRAM

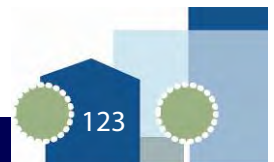
The implementation program undertaken for the Downtown ARP will dictate whether or not the Plan will be considered successful. It is important that tangible results be achieved early on in the implementation process to ensure that interest in the Plan is maintained among business owners and residents. While the Plan outlines implementation activities over a long term period, it is intended that a significant number of initiatives be undertaken within the first five years of the program. It should be noted that any proposals for the provision of public improvements are subject to City Council's approval through the annual Capital Budget process.

The success of any downtown revitalization program is dependent on a partnership approach to improvements. It is not reasonable to assume that either the public or the private sector will shoulder all of the financial responsibilities for identified initiatives or upgrades. While the City will take the lead role in the provision of improvements to public lands and infrastructure in the area, the implementation program for the Downtown ARP is based on the assumption that both the City and land/business owners in the Plan area will share certain financial responsibilities to ensure the successful rejuvenation of the area.

The implementation program is divided into: immediate or "quick wins" initiatives (i.e. those that can be undertaken immediately, even prior to the adoption of the Plan); Year 1 initiatives (i.e. those that should be undertaken in 2009); Years 2-5 initiatives (i.e. those that should be undertaken between 2010 and 2014); and Years 6+ initiatives (i.e. those that should be undertaken starting in 2015 and onwards).

However, as opportunities and issues facing the Downtown evolve over time, and to adjust to emerging market conditions, specific initiatives identified in the implementation program may be accelerated or deferred in particular circumstances, in consultation with affected land/business owners, stakeholder groups, and the wider community. Overall, to keep the Plan and the implementation program relevant, it is recommended that the Plan be reviewed by City Council every five years, and the implementation initiatives list annually.

The initiatives identified for the implementation program of the Downtown Plan are shown, in no particular order of importance, on the table on the following pages. These initiatives are suggested action items for the City and associated stakeholders to undertake. The implementation of specific initiatives will depend on approval by City Council or other affected groups, and appropriate budget and staffing resources being made available.



**Table 14-1 Downtown Area Redevelopment Plan Implementation Program Initiatives**

No.	Initiative	Precinct	Timeline	Responsibility	Funding Source
1.	Formal adoption of Downtown ARP and guidelines	All	Immediate	City Council, City Administration	Project budget
2.	Establish Committee to act as Plan liaison and champion	All	Immediate	City Council, City Administration, Downtown Redevelopment Advisory Committee (DRAC)	City operating budget
3.	Adequate City staff support for Downtown initiatives	All	Immediate	City Council, City Administration	City operating budget
4.	Initiate Downtown mural program	All	Immediate	City Council, City Administration, DRAC	City capital budget
5.	Ambience lighting on street trees and light poles	Core Commercial, Civic, Old Fort/Open Space	Immediate	City Council, City Administration	City capital budget
6.	Ambience lighting on store fronts	Core Commercial, Civic, Live/Work, Northeast Transition	Immediate	Land/business owners	Land/business owners
7.	More scheduled events and activities in Downtown	Core Commercial, Civic, Old Fort/Open Space	Immediate	City Council, City Administration, DRAC, Downtown Business Association (DBA)	City operating budget, DBA



No.	Initiative	Precinct	Timeline	Responsibility	Funding Source
8.	Directional signage to Downtown on Highways 15 and 21 and other key roadways	Outside of Plan area	Immediate	City Council, City Administration	City capital budget
9.	Streets/back lane clean-up	All	Immediate	City Council, City Administration	City operating budget
10.	Advertising about Downtown Plan and implementation initiatives	All	Immediate	City Council, City Administration, DRAC, DBA	City operating budget, land/business owners
11.	Complete branding/marketing strategy for Downtown	All	2009	City Council, City Administration, DRAC	City operating budget
12.	Create Downtown Coordinator position to act as key contact in City Administration for Downtown-related matters	All	2009	City Council, City Administration	City operating budget
13.	Develop incentive programs for façade improvements to buildings and new residential developments	Core Commercial, Live/Work, Civic, Mall Redevelopment. Northeast Transition, 99 th Commercial	2009	City Council, City Administration	City operating budget



No.	Initiative	Precinct	Timeline	Responsibility	Funding Source
14.	Prepare a Financial Strategy/Business Plan for the Downtown to identify methods of funding improvements	All	2009	City Council, City Administration	City operating budget
15.	Undertake a Downtown Parking Assessment and develop strategy for long-term parking needs in the Downtown	All	2009	City Council, City Administration, DRAC, DBA	City operating budget
16.	Signage/banner program	All	2009	City Council, City Administration, DRAC	City capital budget, land/business owners
17.	Design concept and detailed engineering work for 100 Avenue improvements	Core Commercial, Civic, Northeast Transition, Old Fort/Open Space	2009	City Council, City Administration, DRAC	City capital budget
18.	Amendments to Land Use Bylaw, other planning documents to reflect ARP policies	All	2009	City Council, City Administration	City operating budget
19.	Support the re-establishment of a Downtown Business Revitalization Zone (BRZ) to assist in guiding activities	All	2009	City Council, City Administration, DRAC, DBA	City operating budget, land/business owners



No.	Initiative	Precinct	Timeline	Responsibility	Funding Source
20.	Install planters and hanging flower baskets along sidewalks	All	2009	City Council, City Administration, DRAC	City capital budget, land/business owners
21.	Design concept for City Hall Plaza project	Civic	2009	City Council, City Administration, DRAC	City capital budget
22.	Consideration of City-led redevelopment project on City-owned lands to highlight intent of ARP	Core Commercial, Civic	2009	City Council, City Administration, DRAC	City operating budget
23.	Old Courthouse 100th Anniversary celebrations	Old Fort/Open Space	2009	City Council, City Administration, DRAC, Historic Resources Advisory Board	City capital budget
24.	Detailed inventory of heritage buildings/resources in Downtown	All	2009	City Council, City Administration, Historic Resources Advisory Board	City capital budget
25.	Discussions with landowner regarding redevelopment of Fort Mall site	Mall Redevelopment	2009	City Council, City Administration, DRAC	City operating budget
26.	One-day street festival/dance	Core Commercial	2009	City Council, City Administration, DRAC	City capital budget



No.	Initiative	Precinct	Timeline	Responsibility	Funding Source
27.	Extension of Farmer's Market booths into core area	Core Commercial, Old Fort/Open Space	2009	City Council, City Administration, DRAC, DBA	City operating budget, DBA
28.	Clean-up of vacant lots	All	2009	City Council, City Administration, DRAC, DBA	City capital budget, land owners
29.	Information and signage regarding specific Plan initiatives in strategic locations	All	2009	City Council, City Administration, DRAC	City operating budget, DBA
30.	Develop program to provide youth activities in the area to assist in curbing vandalism	All	2009	City Council, City Administration, DRAC, DBA, RCMP	City operating budget
31.	Complete 100 Avenue improvements	Core Commercial, Civic, Northeast Transition, Old Fort/Open Space	2010	City Council, City Administration	City capital budget, land owners
32.	Consider locations for and install entrance features into the Downtown	All	2010 - 2014	City Council, City Administration, DRAC, DBA	City capital budget, land/business owners
33.	Streetscape upgrades on 101 Street, 102 Street, 103 Street, and 104 Street that extend elements of the 100 Avenue improvements	Civic, Core Commercial, Live/Work, Mall Redevelopment, Northeast Transition	2010 - 2014	City Council, City Administration, DRAC	City capital budget, land owners



No.	Initiative	Precinct	Timeline	Responsibility	Funding Source
34.	Tree planting program (also linked to streetscaping upgrades identified in Initiative 31 above)	All	2010 - 2014	City Council, City Administration, DRAC	City capital budget, land/business owners
35.	Development of City Hall Plaza project and associated streetscaping improvements and public open space/plaza or promenade	Civic	2010 - 2014	City Council, City Administration	City capital budget
36.	Selection and installation of consistent street furniture elements	All	2010 - 2014	City Council, City Administration, DRAC, DBA	City capital budget, land/business owners
37.	Identify programs and specific opportunities for affordable housing units to be provided	Live/Work, Mall Redevelopment, Northeast Transition	2010 - 2014	City Council, City Administration, DRAC	City operating budget, outside funding programs
38.	Identify and construct improved pedestrian linkages within the Downtown and between surrounding areas, as identified on Figure 8 - Transportation and Circulation	All	2010 - 2014	City Council, City Administration	City capital budget



No.	Initiative	Precinct	Timeline	Responsibility	Funding Source
39.	Façade improvements to the Field's store building to illustrate design guidelines intent for the area	Core Commercial	2010 - 2014	City Council, City Administration, DRAC	City capital budget
40.	Consider the creation of a Downtown Investment Fund to contribute towards the costs of select improvements	All	2010 - 2014	City Council, City Administration	City operating budget
41.	Explore opportunities to provide enhanced public transit service within Fort Saskatchewan and to and from other centres in the Capital Region, using the Downtown as a transit hub	All	2010 - 2014	City Council, City Administration, DRAC	City operating budget
42.	Streetscape upgrades on 108 Street between 98 Avenue and 99 Avenue, and on 100 Street between 99 Avenue and 100 Avenue	Mall Redevelopment, 99 th Commercial	2010 - 2014	City Council, City Administration, DRAC	City capital budget, land/business owners
43.	Develop and install wayfinding signage system identifying key sites and features of the Downtown	All	2010 - 2014	City Council, City Administration, DRAC	City capital budget



No.	Initiative	Precinct	Timeline	Responsibility	Funding Source
44.	Rehabilitation of City-owned parking lot on the east side of 102 Street, between 99 Avenue and 100 Avenue	Core Commercial	2010 - 2014	City Council, City Administration	City capital budget
45.	Develop a Public Art Program that would require 1% of the constructions costs of new developments to be provided towards the purchase and installation of public art on a site	All	2010 - 2014	City Council, City Administration	City operating budget
46.	Conduct assessment of and consider expansion/ improvements to the Public Library facility	Civic	2010 - 2014	City Council, City Administration, Fort Saskatchewan Public Library Board	City capital budget
47.	Consider opportunities for youth recreation activities in the Downtown core	All	2010 - 2014	City Council, City Administration	City operating budget



No.	Initiative	Precinct	Timeline	Responsibility	Funding Source
48.	Creation of a bicycle route extending from the multi-use trail linkage to the east of the Downtown along Lowe Avenue, extending north along 108 Street, west on 100 Avenue, and south on Sherridon Drive to 99 Avenue	Mall Redevelopment, Live/Work, Core Commercial, Civic, Old Fort/Open Space	2015+	City Council, City Administration, DRAC	City capital budget
49.	Explore opportunities for the provision of a district heating/cooling system or geothermal system for portions of the Downtown	Core Commercial, Civic, Mall Redevelopment	2015+	City Council, City Administration	
50.	Explore opportunities for redevelopment of the Post Office site	Civic	2015+	City Council, City Administration	
51.	Conduct assessment of and consider improvements to the Pioneer House facility	Civic	2015+	City Council, City Administration	
52.	Consider land assembly opportunities for the provision of public open space or other uses	Core Commercial, Live/Work, Mall Redevelopment, Northeast Transition	2015+	City Council, City Administration	



No.	Initiative	Precinct	Timeline	Responsibility	Funding Source
53.	Continue implementation of overall Museum, Old Fort, and Fort Centre Park Master Plans	Old Fort/Open Space	2015+	City Council, City Administration	
54.	Explore opportunities for enhancements to the CN Rail Train Station visitor experience	Old Fort/Open Space	2015+	City Council, City Administration	
55.	Extend streetscape improvements on 100 Avenue west to Sherridon Drive	Old Fort/Open Space	2015+	City Council, City Administration	
56.	Explore opportunities for street-oriented redevelopment along 99 Avenue	99 th Commercial	2015+	City Council, City Administration	
57.	Establish a summer storefront location for a RCMP "Downtown Beat Patrol"	Core Commercial	2015+	City Council, City Administration, RCMP	